

6th Street Corridor Master Plan

GLENWOOD SPRINGS, CO
FINAL PLAN - JUNE 1, 2017



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1. Introduction

6TH STREET CORRIDOR PLAN

PLAN PURPOSE

The 6th Street Corridor Plan presents both a vision and action plan for the North Glenwood neighborhood. It serves as a blueprint for both public and private investment:

- **The city will use the plan as a way to prioritize public investment** in infrastructure such as streets, parking and utility upgrades, as well as placemaking elements like streetscapes, plazas and park improvements.
- **The city will also use the plan as a tool to evaluate future development proposals** of properties within the project area.
- **Landowners contemplating new development or redevelopment should consult this plan** to ensure that development concepts are in-sync with the vision and goals of this plan.

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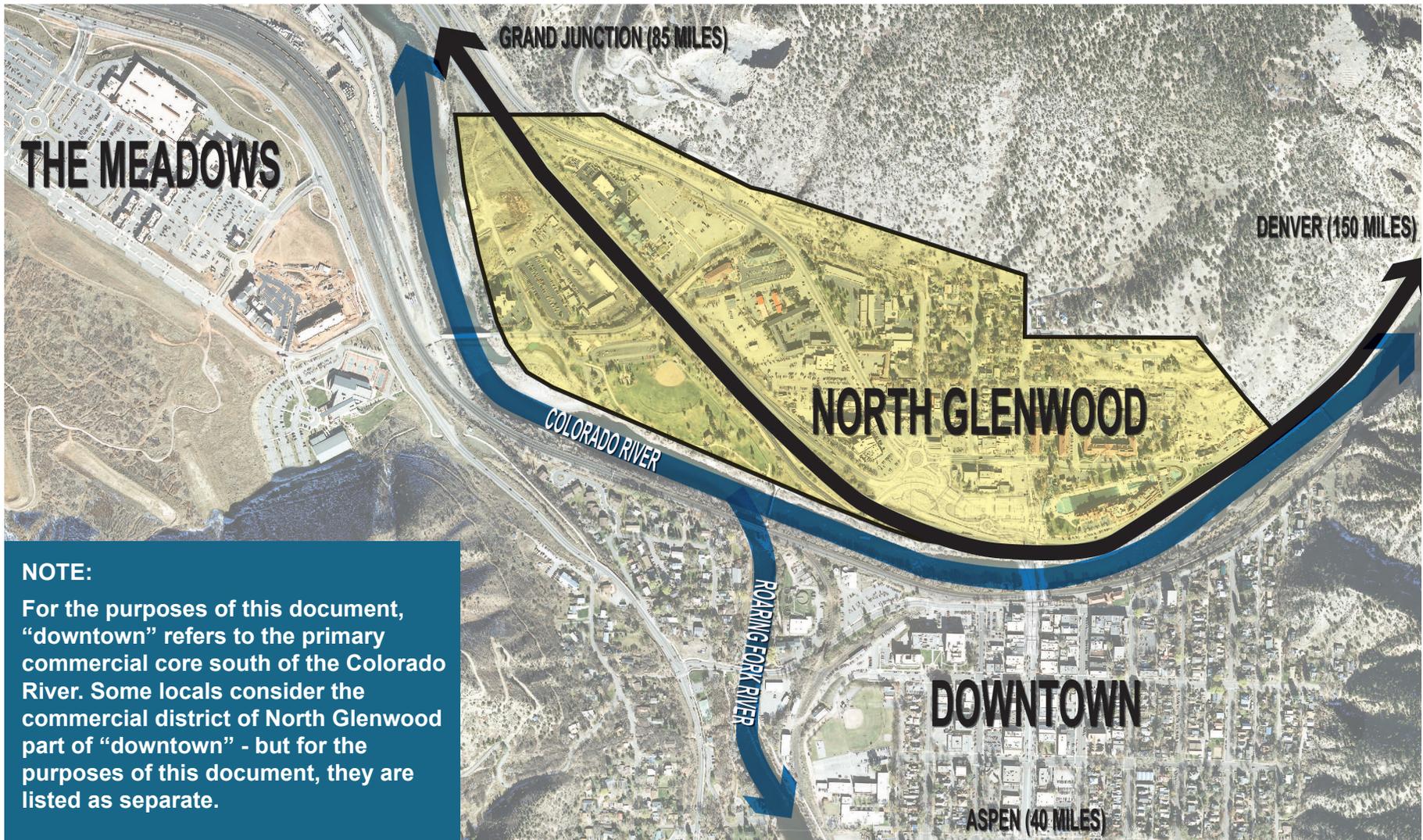


Figure 1 - Context Map

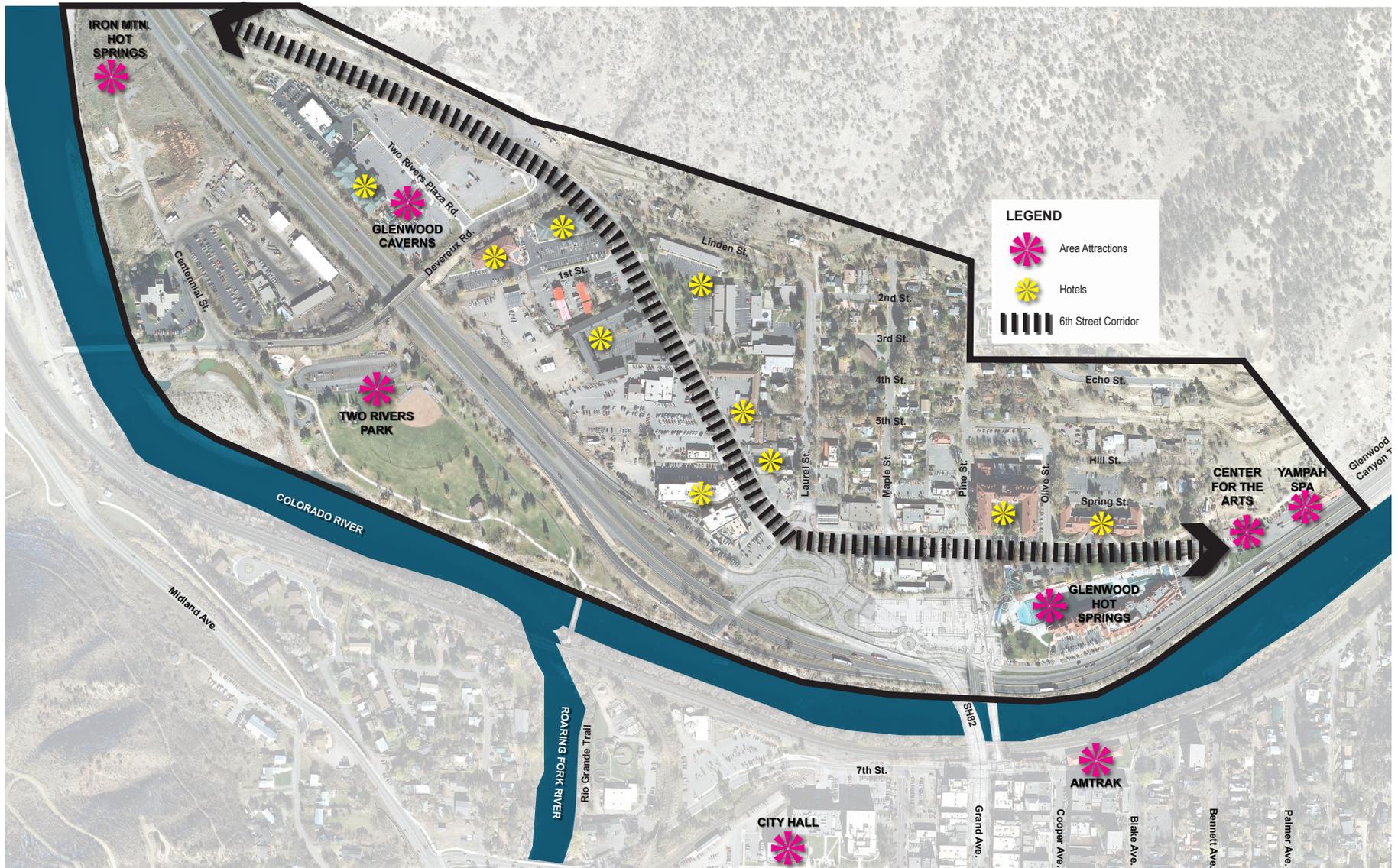


Figure 2 - 6th Street Corridor Study Area & Attractions

OUTREACH AND ENGAGEMENT

The 6th Street Corridor Plan involved extensive public and stakeholder outreach including numerous meetings with a City Council-appointed Project Steering Committee (PSC).

STAKEHOLDERS

The project kicked off with one-on-one interviews with property and business owners in the study area to learn about their long-term visions for their properties and businesses, as well as their desires for the entire corridor and surroundings.

PUBLIC ENGAGEMENT

Three public open houses were held over the course of the planning process. These sessions provided interactive exercises designed to engage and solicit feedback from the public in fun and engaging ways.



Stakeholders were engaged in one-on-one meetings with the design team.

PROJECT STEERING COMMITTEE

A 16-member PSC with diverse representation from key City boards and commissions, area business owners, and neighborhood residents was appointed by City Council to serve as a sounding board for the design team. They were engaged during critical decision-making processes to help guide the plan concepts and recommendations, and to represent the various views and interests of property owners along the corridor.

CITY STAFF

The consultants worked closely with city staff, including multiple work sessions and bi-weekly team conference calls. Staff was involved in the creation of plan concepts and provided critical feedback throughout the process.

CITY COUNCIL

Finally, City Council work sessions were also held to update city leaders on the process and critical elements of the plan.



The Project Steering Committee was engaged during critical decision-making processes.



Open Houses provided interactive exercises designed to engage and solicit feedback from the public.

EXISTING LAND USES

TOURISM-RELATED BUSINESSES

The study area includes major Glenwood Springs tourist destinations and tourist-related businesses, such as:

- Glenwood Hot Springs
- Yampah Spa & Vapor Caves
- Glenwood Caverns Adventure Park Tram
- Iron Mountain Hot Springs
- Rafting & outdoor adventure companies

LODGING

Concentrated along the 6th Street corridor, primarily west of Laurel Street, are many of Glenwood Springs' hotels and motels:

- Glenwood Hot Springs Lodge
- Hotel Colorado
- Ramada Inn
- Silver Spruce Motel
- Holiday Inn Express
- Hampton Inn Glenwood
- Hotel Glenwood Springs
- Best Western Antlers
- Glenwood Motor Inn
- Starlight Lodge



Glenwood Hot Springs is a major tourist destination.



The Yampah Spa and Hot Springs are located at the east end of 6th Street.



Iron Mountain Hot Springs is located along the Colorado River on the west end of the corridor.



The Glenwood Caverns Adventure Park is located on the mountainside above North Glenwood. It is accessed by the tramway on 6th Street.



The Holiday Inn Express is located on First Street, near Highway 6 and Devereux Road.



The Ramada is located near the intersection of 6th and Laurel Streets.



Older hotels such as the Starlight Lodge and Silver Spruce Hotel are also local hotels on the corridor.

COMMERCIAL

Other commercial land uses within the study area include offices, retail, restaurants, spa and beauty salons, an automobile dealership, and automotive service and repair facilities. Like the hotels & motels, most of these businesses front onto 6th Street.

RESIDENTIAL

A compact residential neighborhood sits on the south-facing hill above 6th Street, and east of Laurel Street. The neighborhood consists of mostly single-family homes, some dating from the late 1800s and early 1990s. This section of north Glenwood was part of the original 1885 town site, which also included the downtown area south of the Colorado River to 12th Street. Just like the downtown district, the streets in north Glenwood are aligned on a traditional north-south, east-west grid, but, unlike downtown, they are about 1/2 the width of those south of the Colorado River- which lends to the compact feel of the neighborhood.



Some mixed use buildings are present along 6th St.

VACANT LAND

One of the largest commercial parcels in the study area is the 2.8 acre former Bighorn Toyota lot which sits on the south side of West 6th Street, and which currently serves as temporary parking for the Glenwood Hot Springs and for river rafting customer pick-up and drop-off.

SERVICE PROVIDERS

CDOT and CenturyLink also own property in the area. CDOT's Region 3 offices are located on a 6-acre parcel between I-70 and the Colorado River along Devereux Road and the CenturyLink property is located at the terminus of 1st Street, adjacent to I-70. It serves as a regional hub for CenturyLink maintenance operations and the small buildings on the property are used mainly for storage.

PUBLIC OPEN SPACE

Two Rivers Park, Glenwood's largest regional park, is located within the study area at the confluence of the Colorado and Roaring Fork Rivers and serves as a major destination for events and summer river sport activities. The 22-acre park includes a baseball field, amphitheatre, sheltered picnic areas, restrooms, a playground, and a skate park.



North Glenwood neighborhood has historic character.



The former Bighorn Toyota building is vacant. The lot is currently used for Glenwood Hot Springs parking and for river rafting pick-up/drop-off.



Subway is one of two drive-through fast food services in the study area.

HISTORIC RESOURCES

North Glenwood has a rich historic fabric. The following is a summary of the historic status and/or eligibility status for buildings in the study area:

BUILDINGS ON THE STATE OR NATIONAL REGISTERS OF HISTORIC PLACES

*SRHP = State Register of Historic Places

*NRHP = National Register of Historic Places

115 5th Street	Shelton-Holloway House	SRHP
601 6th Street	Glenwood Springs former Hydroelectric Plant	NRHP
526 Pine Ave.	Hotel Colorado	NRHP

BUILDINGS ELIGIBLE FOR STATE OR NATIONAL REGISTERS OF HISTORIC PLACES

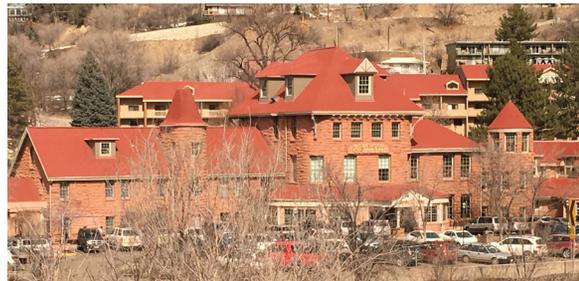
515 Maple Street	Modern style residence	Survey 2003
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BUILDINGS ELIGIBLE FOR CONTRIBUTING TO A LOCAL HISTORIC DISTRICT

401 River Street	Hot Springs Bathhouse, Natatorium, Yampa Spring	Survey 1998
201 5th Street	Queen Anne Cottage	Survey 2002



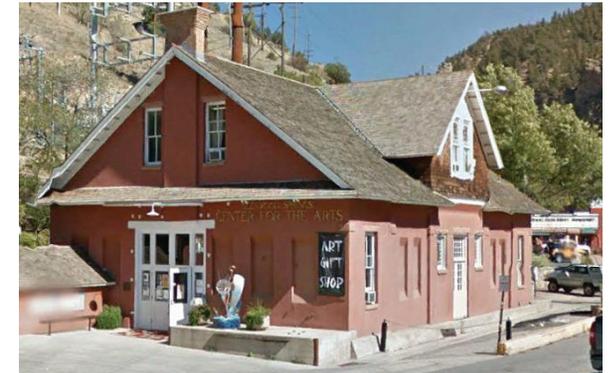
Hotel Colorado



Hot Springs Bathhouse



Shelton-Holloway House



Former Hydroelectric Plant (Center for the Arts)



201 5th Street - Queen Anne Cottage

NOTE:

Currently, there are no registered historic districts in Glenwood Springs, although the area east of Pine Street and across the Colorado River to 7th Street has been determined eligible for inclusion on the National Register of Historic Places. It includes the Hotel Colorado, the Hot Springs, the city-owned Hydroelectric Plant and the train station.

CURRENT ZONING

The current zoning for the study area includes the following zone districts (refer to Figure 3 on page 8):

- **C/1 - Limited Commercial**
- **C/3 - General Commercial**
- **C/4 - Resort District**
- **R/1/6 - Single Family Residential**
- **R/1/20 - Single Family Residential**
- **R/2 - Limited Multifamily Residential**
- **R/3 - Multifamily Residential**
- **R/4 - Residential Transitional**
- **I/1 - General Industrial**

The area outlined with a dashed red line (C/1, C/3 and C/4) indicates the area where this plan envisions most future redevelopment occurring. Because some of the current zone district standards do not align with the redevelopment vision, this plan recommends revising existing zoning standards. The residential zoning is not envisioned as changing.

As part of this project, design standards and guidelines are being developed for the area inside the red dashed line that shall be used in cooperation with the new development code. They are provided in Appendix A.

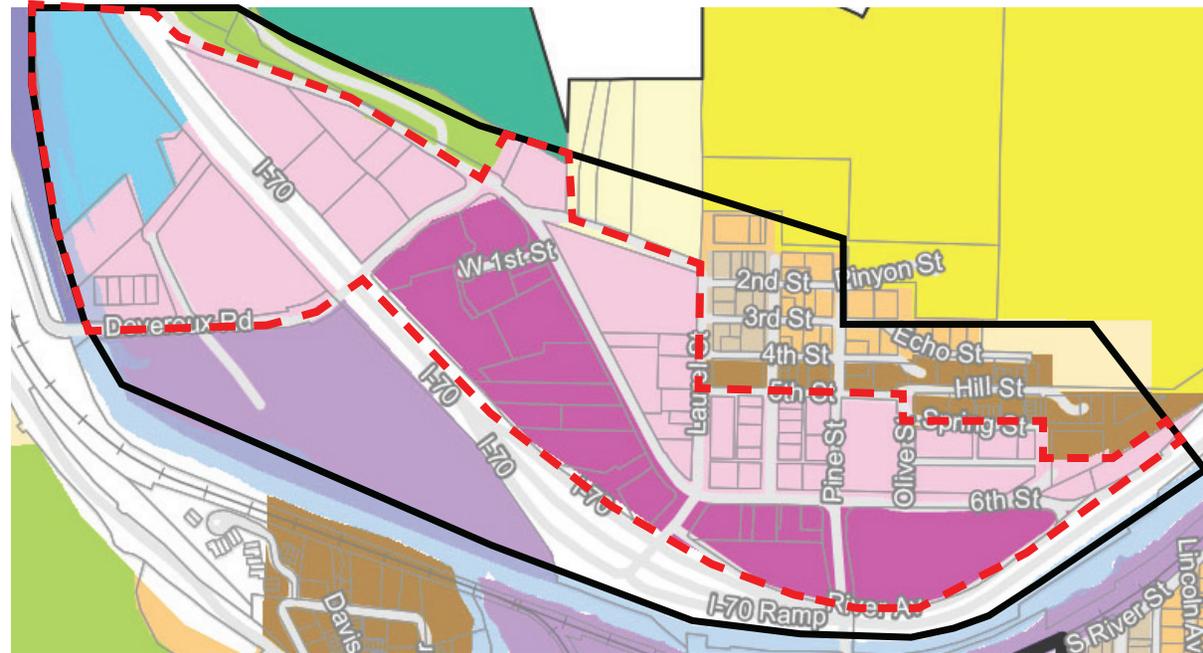


Figure 3 - Current Zoning

Zone Districts	
	C/1 Limited Commercial District
	C/2 Core Commercial
	C/3 General Commercial
	C/4 Resort District
	HP Hillside Preservation
	I/1 General Industrial
	I/2 River Industrial
	I/L Light Industrial and Office
	PUD Planned Unit Development
	R/1/20 Single Family Residential
	R/1/40 Single Family Residential
	R/1/6 Single Family Residential
	R/1/7.5 Single Family Residential
	R/2 Limited Multifamily Residential
	R/3 Multifamily Residential
	R/4 Residential Transitional

NOTE:

In 2016, the City began a process to update the entire development code. This plan was completed prior to the adoption of the development code, however the plan has influence on the proposed zoning actions for the study area. For more information on how the new zoning code relates to North Glenwood and the 6th Street corridor, refer to Appendix (forthcoming).

I-70 INTERCHANGE PROJECT

Glenwood Springs' Exit 116 is undergoing a major reconfiguration, including a new Grand Avenue auto bridge, separate pedestrian bridge, and roundabout that intersects 6th Street in the project area. This major construction project is estimated to be complete in May 2018.

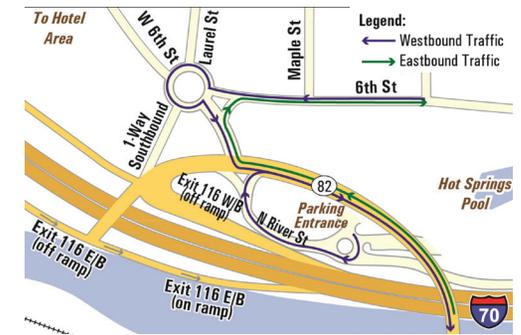
This project opens up both opportunities and challenges for the 6th Street corridor:

- **6th Street from Laurel to Pine Streets** will be transferred from CDOT to City ownership following completion of the Grand Avenue Bridge, which means that the City will be responsible for all future right-of-way maintenance. This provides an opportunity to transform these two blocks from a 4-lane thoroughfare into a quiet, pedestrian-oriented, "main street."
- **Traffic will decrease substantially.** CDOT estimates an 80 percent reduction in traffic between Laurel and Pine streets, as through-traffic will now cross the Grand Avenue Bridge directly from the Laurel roundabout.
- **A new public park or plaza** is proposed to replace the current Grand Avenue Bridge landing at 6th and Pine streets. This new amenity not only will help spur investment along 6th Street, it will provide the area with a prominent gathering space.
- **The new pedestrian bridge** will be separated from the Grand Avenue auto bridge, and includes viewing areas that overlook the Pool and Colorado River. This bridge will provide a pleasant pedestrian connection from downtown to 6th Street.

- **The new roundabout** occurs at the intersection of Laurel and 6th Streets. This proposes a challenge in terms of navigating the new arrangement. Signage will be very important in terms of directing visitors in automobiles to their destinations efficiently. As a pedestrian, the only way to navigate the roundabout is on the north side, so people on the south side of 6th Street trying to get to and from the east and west sides of Laurel, will need to do so by crossing to the north side of 6th and then crossing Laurel. Roundabouts are generally difficult for bicyclists to navigate, and this particular configuration is quite complicated given the amount of traffic that will use it. Therefore, considering bike routes that can bypass the roundabout will be important.
- **Construction of the new bridge** has been disruptive to businesses, as will any future improvements to 6th Street, so being considerate about phasing and timing of construction along the corridor is important.

NOTE:

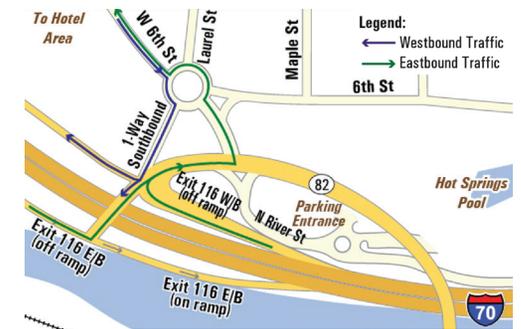
For the purposes of this plan, most concepts are portrayed assuming that the new interchange is in place. This is to avoid having to update the plan once construction is complete.



Traffic to the Pool from downtown will cross the bridge and turn right on 6th Street. To get back to downtown from the Pool, people will have to either take 6th Street back to the roundabout and over the bridge, or take River Street up the ramp and over the bridge.



To get on I-70 from the Pool, the only route is back through 6th Street.



From I-70 to Hotel Row and West 6th Street, people will have to go around the roundabout.



View from West 6th Street showing the new roundabout, Exit 116 interchange and the new bridges to downtown.



View of the new multi-use path and tunnel.



This view shows the circulation to the pedestrian bridge and restrooms along 7th Street.



This aerial view above the new park looking south toward downtown. The pedestrian bridge overlooks can be seen here. The new pool parking lot is shown in the foreground.



View from east-bound I-70 showing the new Grand Avenue Bridge and pedestrian bridge behind.

2. Vision & Primary Goals

6TH STREET CORRIDOR PLAN

6TH STREET VISION

6th Street is a unique blending of local tradition with contemporary appeal. Its historic architecture, iconic tourist destinations, affordable lodging and housing options, eclectic shopping, excellent dining, breweries, and dedication to the arts is what makes 6th Street such a desirable place to live, work and play. The unique appeal is enhanced with its public open spaces and welcoming streetscape. It provides a unique experience, different from downtown, that people of all ages enjoy.

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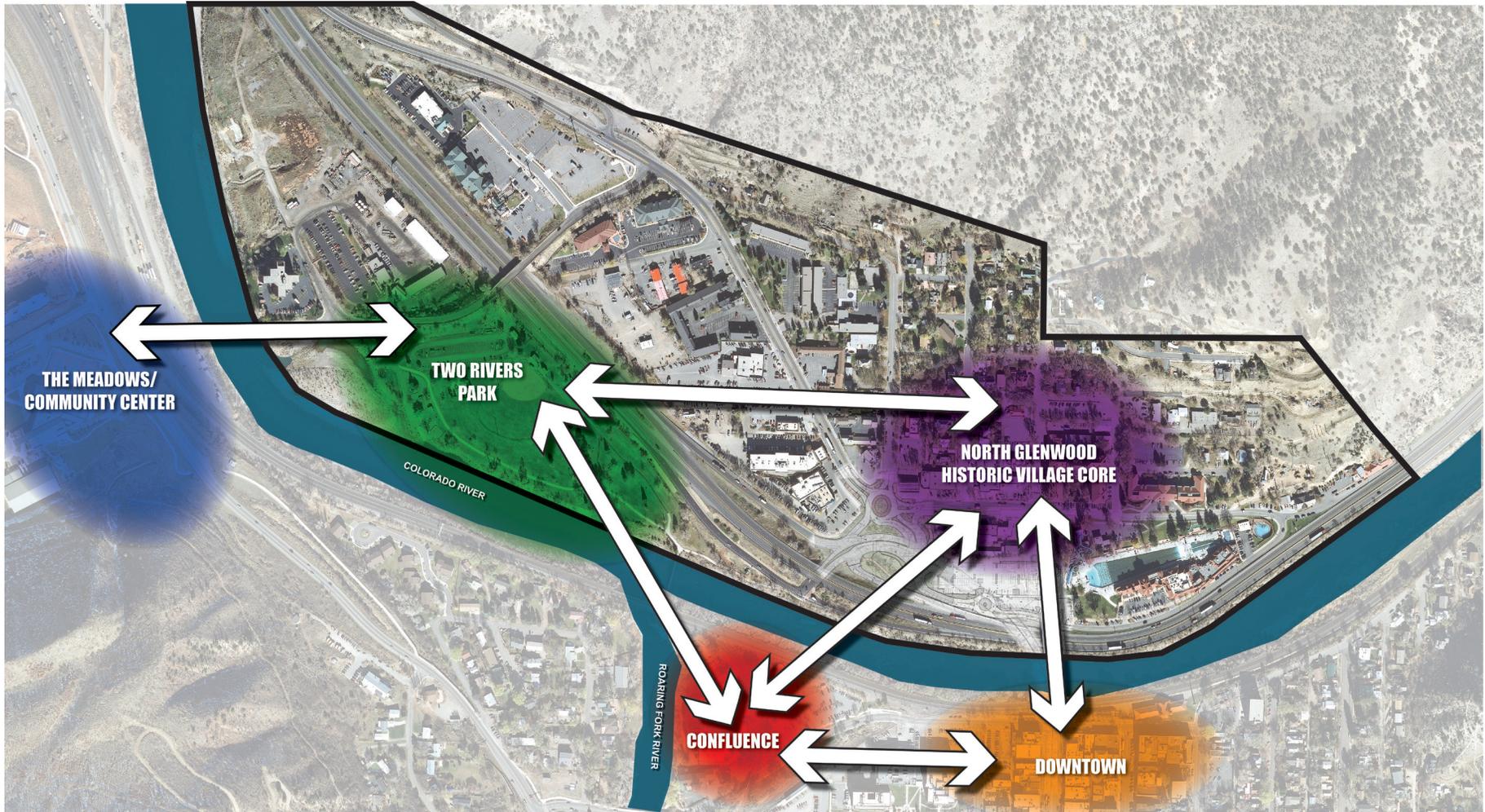


Figure 4 - Major Activity Centers

CONNECTING MAJOR ACTIVITY CENTERS

Although North Glenwood is connected to downtown by vehicular and pedestrian bridges, the neighborhood feels isolated. Interstate 70 and the Colorado River are significant barriers however, they also provide opportunities:

- I-70 provides **visibility** to businesses and attractions along the 6th Street corridor; and
- The Colorado River is an **amenity** that brings visitors to Glenwood Springs for rafting, kayaking and fishing. These visitors in-turn frequent those businesses and lodging along the 6th Street corridor.

A primary objective of this plan is to better connect the major activity centers in this area (Figure 4):

- **North Glenwood Village Core** – this section of 6th Street, primarily between Laurel and Pine, is envisioned as becoming an extension of downtown, including a mixture of old and new buildings with uses that complement downtown. This area also has significant potential for becoming a pedestrian-oriented “main street” due to a significant decrease in through-traffic from Highway 82 that will be rerouted with the new interchange by 2018. A new plaza at the north landing of the pedestrian bridge will be a focal point for this neighborhood. Once constructed, it will help spur other public and private investment along the street.

- **Two Rivers Park** – the largest park in Glenwood Springs, Two Rivers Park, is a popular destination for outdoor recreation, live music performances, and river sports. Access to the park for cars as well as pedestrians and bicyclists, is somewhat limited and difficult to navigate. This plan considers opportunities for improving connectivity, parking, and programming for the park.
- **Glenwood Meadows** – this popular shopping district has a regional draw that could bring more business into the 6th Street Corridor, if connections were improved. This plan looks at opportunities for a bridge connection (for autos or bikes) to link the two areas.
- **The Confluence** – the confluence of the Colorado and Roaring Fork rivers, and the demolition of the city’s former wastewater treatment facility, offers tremendous opportunity for redevelopment in this prime location. Many land uses have been considered for this area. Whatever uses occupy the Confluence, linking the area to the 6th Street corridor is a primary goal.
- **Downtown** – providing a seamless connection between downtown and the 6th Street corridor is another important goal. It is less than a quarter-mile (or about a 3-5-minute walk) from 6th Street, across the bridge to the 7th Street shops and restaurants. This plan encourages land uses and streetscape treatments along the 6th Street corridor that coordinate with and complement the downtown.



There is significant opportunity for creating a pedestrian-oriented “main street” along E. 6th due to the decrease in traffic when the new bridge is in place.



Providing a seamless connection between the north and south sides of the river is a primary goal.

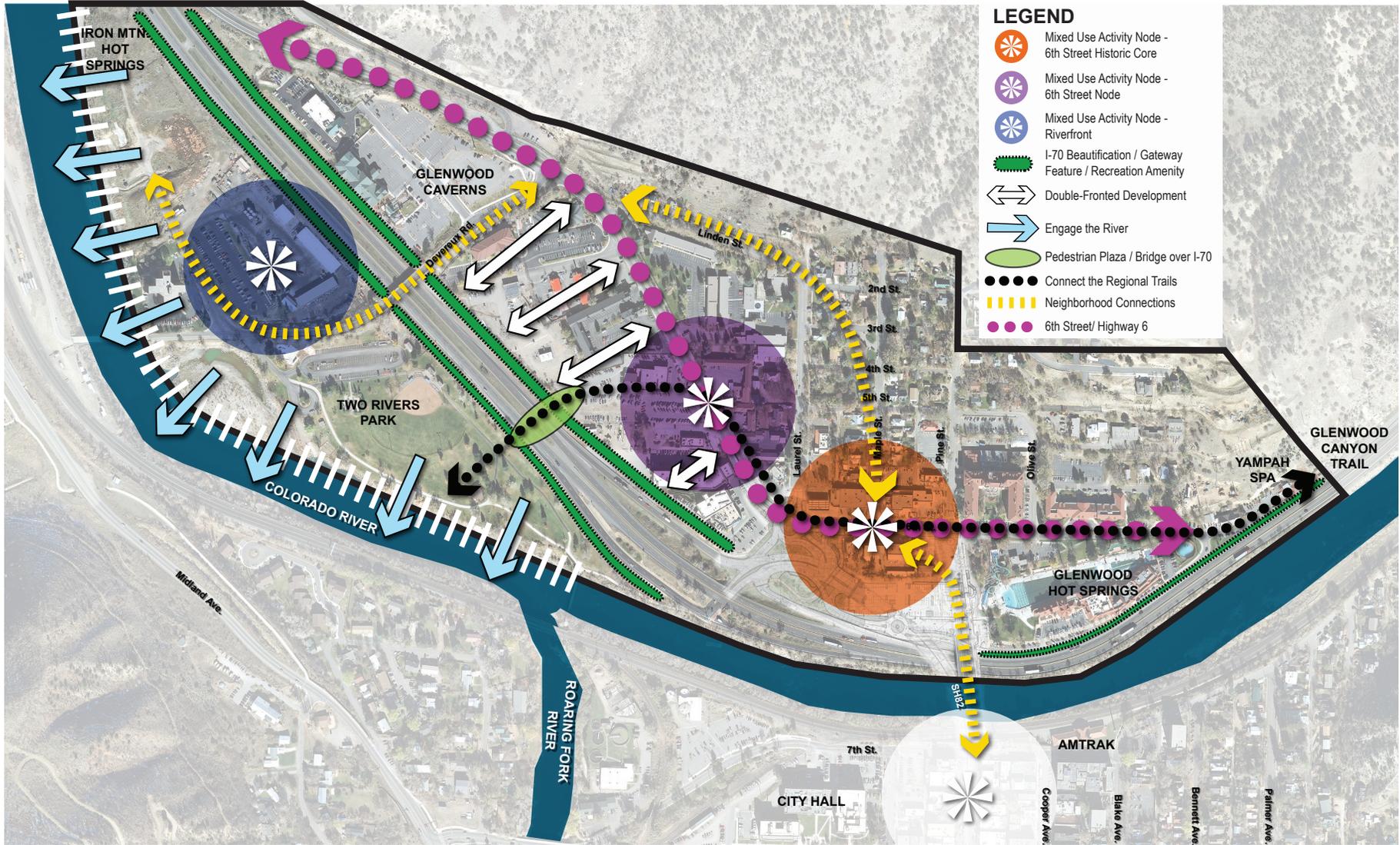


Figure 5 - Primary Goals

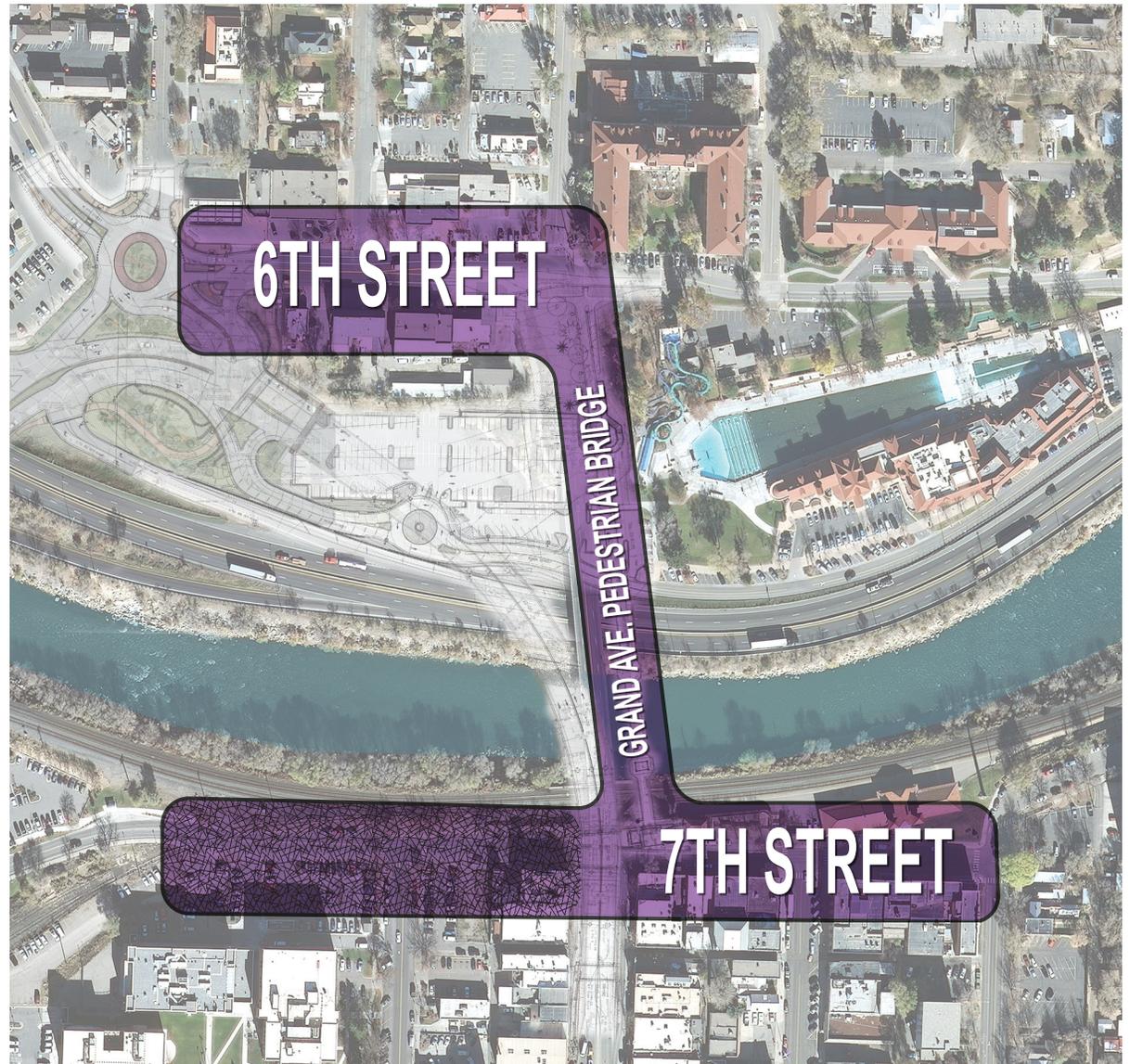
PRIMARY GOALS

Figure 5 represents the “big ideas” and primary goals that this plan addresses. The primary goals are described in more detail on the following pages.

SERVE AS AN EXTENSION OF DOWNTOWN

6th Street should operate as an extension of downtown where residents and visitors pass freely between the two areas, primarily by foot or on a bicycle. Illustrated at right is a concept where the 7th Street restaurants and existing and future uses along 6th Street are seamlessly connected by the Grand Avenue pedestrian bridge. This pleasant walk involves little interaction with cars due to the pedestrian bridge and quaint character of both 7th and 6th Streets, and therefore walking or bicycling should be an obvious choice. Both 7th and 6th Streets should support this vision by providing:

- **Continuous, wide sidewalks** that are universally accessible and provide ample room for small groups of people to walk side-by-side.
- **An inviting landscape treatment** to make the walk more pleasant.
- **Convenient bicycle parking** in highly visible locations.
- **Benches and cafe seating areas** to provide activation and eyes on the street.
- **Signage** to direct people effortlessly to their destinations.



6th Street should serve as an extension of downtown with a seamless connection via the new Grand Avenue Pedestrian Bridge. The hatched area west of the bridge will be incorporated into planning efforts for the Confluence area. Ensure that future land uses and streetscape treatments in this area provide a seamless connection to 6th Street.

ENHANCE THE IDENTITY OF 6TH STREET

Although being viewed as an extension of downtown is desirable, 6th Street should have a **unique identity**. It should offer a different experience, in terms of services and character - complementing, rather than competing with, downtown. Some principles to accommodate this vision include:

- 6th Street should be a “**complete street**” that comfortably accommodates cars, transit, bicyclists and pedestrians.
- It should be **family-friendly** and feel safe and inviting to visitors - offering an experience that is worth coming back for.
- It should present Glenwood Springs as an **outdoor recreation destination**, providing clear connections to the surrounding trails.
- Landscaping should appear more natural and buildings should “**open up**” to the street and sidewalk, with opportunities to bring the outdoors inside.
- Surrounding **views should be protected and celebrated** by incorporating rooftop decks and overlook areas.



Improvements to 6th Street should enhance the identity of the area. This rendering envisions an improved streetscape and new building(s) on the south side of 6th, in the vicinity of Laurel Street.

IMPROVE CONNECTIVITY

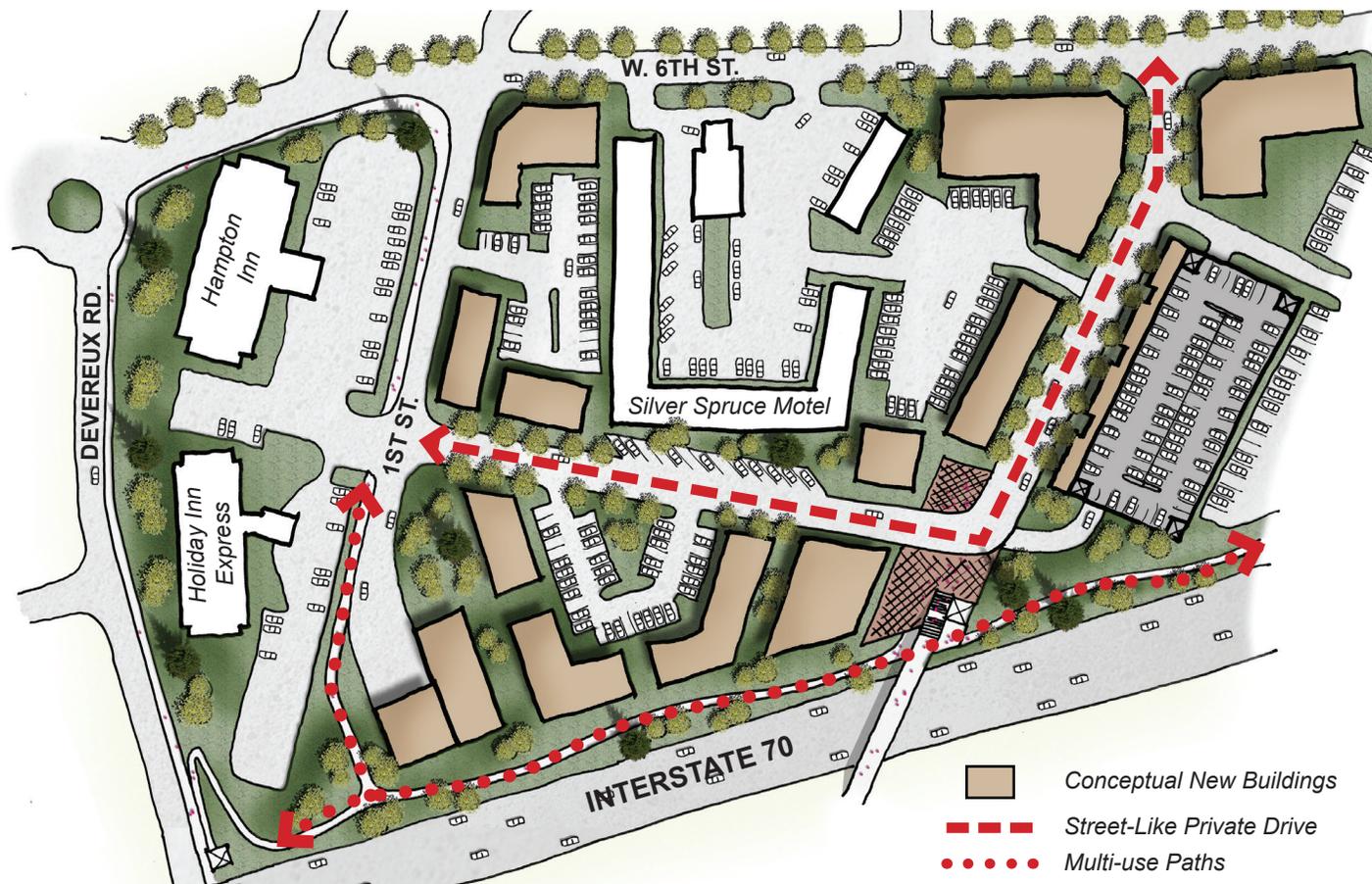
Improved connectivity is needed both within the study area, and to link the study area to surrounding destinations. The following are principles for achieving improved connectivity:

- **Upgrade or add sidewalks** where they do not currently exist to provide a continuous pedestrian network.
- **Implement crosswalks** to facilitate movement across 6th Street, especially in West 6th Street.

- **Break up the mega-blocks.** West 6th Street from Laurel to Devereux consists of two “mega-blocks” on either side of the street with few opportunities to move through them. This can be accomplished by inviting foot and bike traffic with multi-use paths and providing street-like private drives that operate like a street and serve as access to multiple properties rather than just one. The former could be a simple agreement with the property owner and the City to provide access through their property. The latter might demand

a higher degree of cooperation from the property owner, but would greatly benefit the entire area.

- **Install wayfinding signage** that clearly directs people to their destinations.
- **Implement bike facilities** and ample bike parking to encourage this mode of transportation.



A street-like private drive that includes pedestrian facilities could greatly improve the connectivity along West 6th Street and break up the “mega-blocks”.

CREATE NODES OF ACTIVITY WITH A MIXTURE OF NEW USES

The long-term vision for 6th Street is to provide a mixture of uses within a short distance of one another to promote walkability, enhance the identity of the area, and increase economic viability.

It is not feasible to assume the entire mile-long corridor will redevelop with retail on the ground floor and residential or office uses above, nor would that be healthy for the local businesses. Rather, the ideal

scenario is to look at “nodes” of opportunity that could support an influx of new uses, while preserving existing ones, to enliven the entire corridor. These “nodes” would likely occur in areas that are ready (or near ready) for redevelopment: for example, on vacant or underutilized land.

Desirable and appropriate new uses for the corridor, as identified by the public, stakeholders, and the Project Steering Committee include:

- **Mixed-use buildings** with retail on the ground floor and residential units or offices above.
- New **residential housing types** such as townhomes and apartments.
- **A civic use** such as a performing arts center, event center, or fire station.
- More **restaurants and nightlife**.
- **Retail** to support tourists.
- More **lodging** - potentially a high-end hotel.



Proposed “node” of mixed-use activity at the intersection of West 6th Street and proposed new street. Mixed use building is shown on existing Glenwood Motor Inn site as an example of potential development opportunity.

BEAUTIFY THE EDGES

North Glenwood and the 6th Street Corridor are highly visible from I-70, which provides fantastic opportunity to highlight all that it has to offer. Currently, this area does not appear inviting and does not have a sense of “gateway”. This plan encourages beautification and activation of the I-70 corridor by:

- Incorporating **enhanced landscaping** along I-70 to improve the identity of North Glenwood.

- Incorporating **multi-use trails** in public easements along the I-70 corridor to highlight Glenwood Springs as an outdoor recreation destination.
- Expanding **new development** closer to I-70 west of the roundabout. With improved connectivity in this area, development could be “double-sided” - fronting onto both I-70 and West 6th Street.

- Developing a **sense of arrival or “gateway”** should occur near the Vapor Caves. This area is very limited in terms of space, but moving the Glenwood Canyon trailhead to the intersection with improved crossings, bike facilities, and signage and incorporating public art would be very beneficial.



A “gateway” should occur near the Vapor Caves and Center for the Arts with enhanced landscaping, public art, and by moving the Glenwood Canyon trailhead west to the North River Street intersection.

ENGAGE THE RIVER

This plan encourages new programming of Two Rivers Park to better engage and activate the river. Programming elements to improve activation of the park include:

- Incorporating a **beach island area** to encourage summertime activation and to allow people to interact with the river.
- Improving the **boat ramp area** to better facilitate river sports.

- Including **picnic areas** and shelters.
- Building “**overlook**” **areas** for people to watch river sport activities and enjoy the views.
- Provide **hiking and biking trails** throughout the park that connect to 6th Street and the regional trails.

Future developments such as the expansion of Iron Mountain Hot Springs, should look for opportunities to engage and activate the river. Additionally, should CDOT relocate its facility, the roughly 6 acre site could be redeveloped into a mixed use activity node with recreational uses and residential uses that help activate the park and river. A restaurant or brewpub could also benefit the recreational users, spa users, and future residents.



This plan encourages new programming of Two Rivers Park to better engage and activate the river. A shoreline enhancement project is currently underway.

3. Character Areas and Zoning

6TH STREET CORRIDOR PLAN

CHARACTER AREA GOALS

Character is often used to describe the personality of a place. The character of a place is influenced by history, architecture and how a place is used and inhabited. North Glenwood has some areas that are rich in character and others that have the potential to grow in character.

The area of North Glenwood east of Laurel Street is rich in character, as it was platted as part of the original town site in 1885. It has distinct “character areas” within it, some of which are geared towards preservation and enhancing existing character and other areas which have more flexibility.

The rest of the study area does not have a significant engrained character. The section of West 6th Street (west of Laurel Street) is commonly referred to as “Hotel Row” due to the many hotels in the area that support surrounding tourist destinations. The majority of these hotels were built mid-century and are indicative of the “motor inns” of that era. While certain elements are worth preserving/highlighting, the majority of this area does not represent the desired character for this area and new development should respond to the future vision rather than the status quo.

The study area is divided into six “character areas” (see Figure 6). As properties redevelop, the goal is to use this chapter so that new development further enhances the desired character, as portrayed on the following pages.

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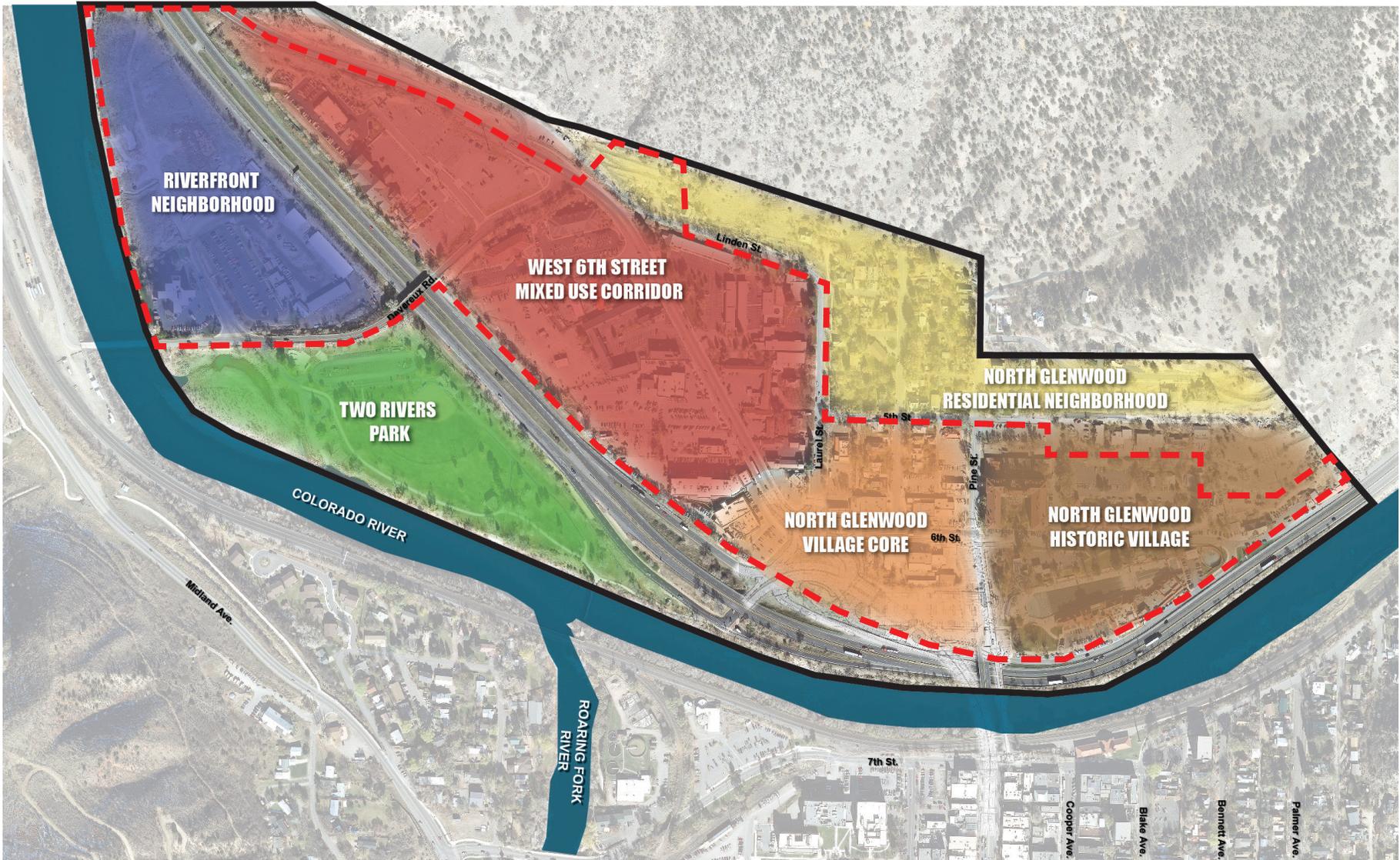
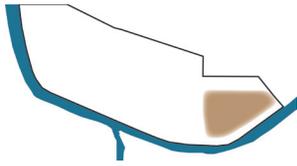


Figure 6 - Character Areas

 indicates where new zoning and design standards and guidelines apply.



NORTH GLENWOOD HISTORIC VILLAGE

The area shown in brown is identified as the North Glenwood Historic Village due to the significant historic structures and character. The following goals apply for this character area:

1) This area is eligible for historic district status and should be pursued in the near term.

There are placemaking and tourism opportunities associated with creating a registered historic district. For example, a “gateway” element at Pine and 6th could serve as an entrance into the historic district. Plaques or kiosks and guided or personal walking tours in this area could help tell the history and promote the area as a place for heritage tourism.

2) New development should be sensitive to the adjacent residential neighborhood.

New buildings should be sensitive to existing, low-scale residential structures to the north by designing with the topography and stepping down in scale and intensity further north.

3) Provide more parking for existing uses.

Many of the existing uses are underparked, which pushes public parking into the residential neighborhood. As redevelopment occurs, new parking facilities should be included to alleviate this burden on the existing residents and to provide a better visitor experience.

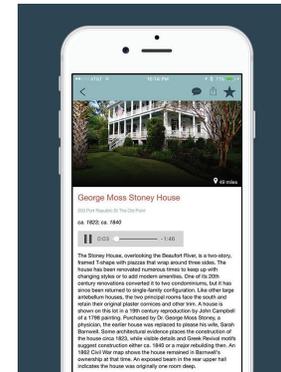
4) New development should respond to the historic character and appear subordinate.

New buildings should respond to the historic context, yet appear of their own time. For example, using similar materials such as red sandstone or red clay roof tiles would be appropriate whereas replicating the Italianate style of architecture of the Hotel Colorado is not appropriate. New buildings should be subordinate, allowing the historic structures like the Hotel Colorado and Hot Springs Bathhouse to stand out.

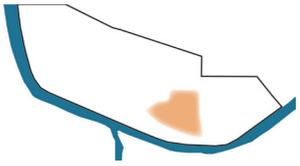
Another characteristic is this area’s significant landscaping and space between buildings. For example, the Hotel Colorado’s courtyard entry and the lawns for the Hot Springs Pool provide a more “natural” experience, as opposed to a more “urban” environment where buildings are side-by-side. New buildings should respond to this characteristic by providing ample landscaped open space.

NOTE:

Special design standards and guidelines for the area within the red dashed line are provided in Appendix A. The design standards and guidelines provide rules and expectations for new development to follow, as a means for achieving the overall vision and desired character for this area. They relate directly to the proposed zoning recommendations at the end of this chapter.



Plaques or kiosks and guided or personal walking tours in this area could help tell the history and promote the area as a place for heritage tourism.



NORTH GLENWOOD VILLAGE CORE

This area is envisioned as becoming an extension of Downtown Glenwood Springs with restaurants, brewpubs, and retail businesses that support the adjacent uses, as well as draw people across the river from downtown. The following principles apply:

1) Maintain a “village-like” character.

New development should respect the quaint, village-like character of East 6th Street. This means respecting the residential neighborhood to the north with sensitive transitions in building mass and scale and using human-scaled design and details. Some ideas for preserving the “village-like” character include:

- Maintaining some of the existing residential structures along Maple Street, perhaps with additions to accommodate contemporary uses. Some of these structures have already converted into commercial uses, and this pattern is supported.
- Transforming the alley north of 6th Street into a pedestrian-friendly place with enhanced lighting, public art, and double-sided commercial buildings.

2) New buildings that face onto 6th Street should take on characteristics similar to downtown buildings to create a “main street” appeal.

Architecturally, new buildings facing 6th Street should take on characteristics inherent in downtown buildings, such as:

- Buildings constructed to the street edge with storefront

windows on the ground floor for displaying goods or services inside.

- Usually more than one-story (but not more than three) and include offices or residential units on the upper floors (i.e. mixed use).
- Entrances to buildings are recessed and sometimes include awnings overhead to provide further protection from the elements whereas corner buildings orient to both streets, and often include a 45-degree notch in the building to address the corner.
- Masonry as a primary building material.
- Structures are built side-by-side and a repetition of unique structures can be perceived, but with aligned common elements such as floor and cornice lines.

3) Blend historic with contemporary styles.

While it is desirable for new buildings to take on similar characteristics of historic downtown buildings, they should not replicate historic styles of architecture. New buildings should appear of their time and place and respond to contemporary trends and needs. For example, a mixture of traditional materials such as masonry and stone is desired, as well as contemporary materials such as glass and steel.

Adaptive reuse of some buildings is also appropriate to achieve this goal and can help tell the story of a place.

4) Public parking is desired.

Public parking in this area is desired to support redevelopment efforts. A parking structure should be pursued, either as a public initiative or a joint partnership. Parking requirements for buildings in this area should be waived, or greatly reduced, to support new development and create a compact, “main street” and village-like appeal rather than having parking requirements take up valuable land.



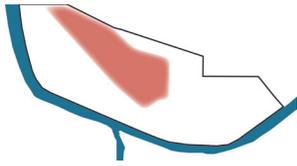
Maintaining some of the existing residential structures along Maple Street is desired.



Architecturally, new buildings facing 6th Street should take on characteristics inherent in downtown buildings.



Adaptive reuse of some buildings is appropriate to blend historic and contemporary styles.



WEST 6TH STREET MIXED USE CORRIDOR

This area is envisioned as increasing in intensity -- keeping the lodging and entertainment focus while adding in new uses for visitors and residents to enjoy and to create a more pedestrian-friendly experience along 6th Street. The following principles apply:

1) Create a pedestrian-friendly 6th Street.

The existing hotels, most of which were built in the 50s or 60s, are indicative of the “motor inns” of that era – with exterior circulation and buildings that orient to parking lots instead of the street. While there is a unique character that can be maintained or enhanced with these structures, it is also very important for this portion of 6th Street to become more pedestrian-oriented. Enhanced landscaping, street trees, lighting, and small pocket parks or plazas are desired to enliven and activate the street - as well as new uses.

2) Add new uses.

While the primary land use will remain focused on lodging, there is opportunity to mix in retail, restaurants, and perhaps even a civic use to help activate this area. Due to the location of this character area, new land uses could be more intense, perhaps with taller buildings to take advantage of views and increased density.

3) Public Parking Structure

A new public parking structure is envisioned adjacent to the interstate to support new development. This structure should be designed as integral into the fabric of West 6th Street with clear connections to it. It should serve multiple uses, including events at Two Rivers Park, as well as general activities along the 6th Street corridor. Including active uses is important, such as retail on the ground floor or a climbing wall.

4) New development should positively contribute to the street edge.

New development should orient to 6th Street with activities that support the hotel occupants and visitors as well as residents. Providing “nodes” of activity where new buildings engage and activate the sidewalk edge is desired. In other locations, enhanced landscaping or shared open space is desired.

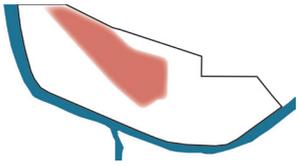
- New buildings should be placed closer to the street to better enclose the street space and make a more pleasant pedestrian experience.
- Small plazas or pocket parks, or other semi-public space adjacent to buildings is encouraged to mimic the existing “open” character. A smaller version of the parklike setting in front of the Antlers Hotel would be welcomed.
- Parking should be subordinate to new buildings, located either to the side or behind new structures.
- Where buildings are not constructed to the street edge encourage landscaped strip or street trees adjacent to the public sidewalk.
- Taller buildings are appropriate along this section of the corridor - perhaps a few more than 4 stories.



Enhanced landscaping, street trees, lighting, and small pocket parks or plazas are desired to enliven and activate the street.



Including active uses “wrapping” the parking structure is encouraged, such as retail on the ground floor or a climbing wall. This parking structure in Chattanooga, TN is wrapped with climbing wall panels that create a unique architectural appearance from afar, as well as up close.



4) Consolidate access to properties to reduce curb cuts.

Each individual parcel of property along this stretch of 6th Street includes at least one if not multiple access points. These curb cuts are convenient to automobiles, but they make walking undesirable as they interrupt sidewalks, making pedestrians feel like they have to be “on guard” at all times.

This area should seek to consolidate access along 6th Street. Wherever feasible, adjoining properties should share auto access to reduce the amount of curb cuts along the street and help create a safer and more pedestrian-friendly environment.

5) Enhance connectivity.

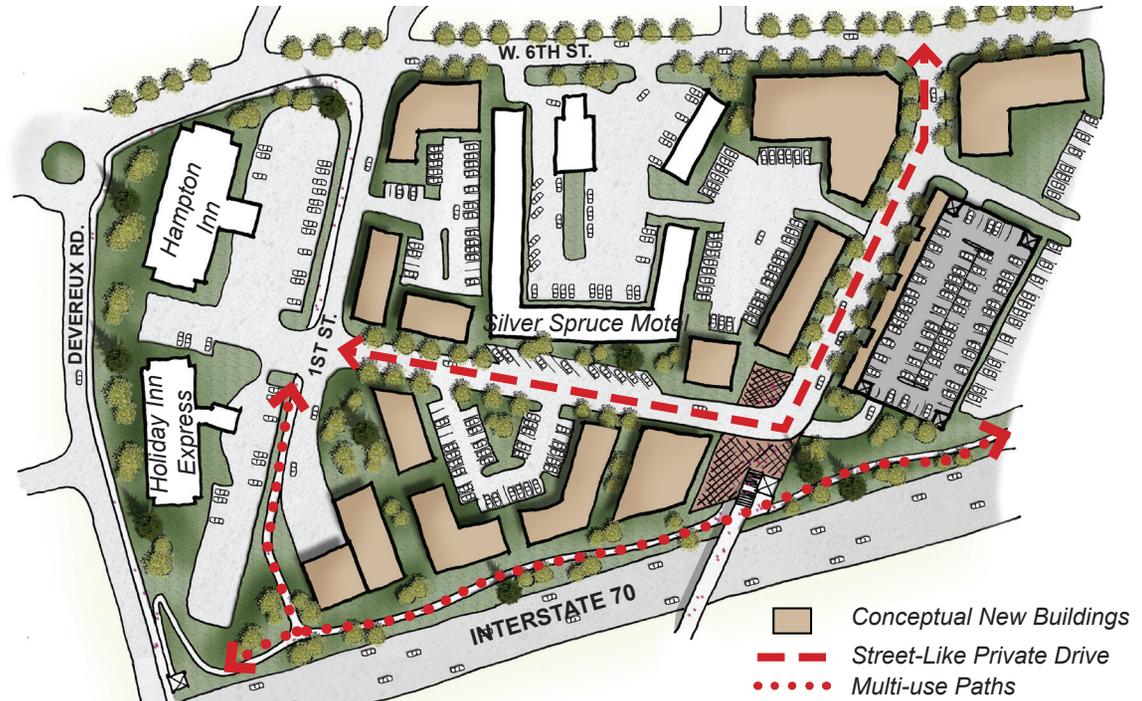
This area is made up of “mega blocks” along 6th Street from Laurel Street to Devereux Road with no dedicated public access points or permeation in between. This makes connecting to the surrounding character areas difficult, as well as simply crossing from one side of the street to the other. A pedestrian and bicycle connection is envisioned that connects from Laurel Street to 6th Street aligning with 5th Street to break up the northern mega block. On the southern mega-block, a street-like private drive and multi-use paths are envisioned to enhance connectivity and provide additional developable land.

6) Provide a “gateway” feature at 6th Street and Devereux Road.

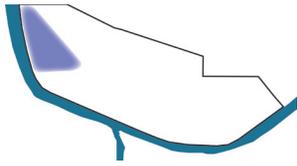
The intersection of West 6th Street and Devereux Road should include a “gateway” feature to present an entry into North Glenwood from West Glenwood. A roundabout is envisioned to better control traffic in the area coming to and from Glenwood Caverns. This could also serve as a “gateway” feature with signage, public art and enhanced landscaping.



A pedestrian and bicycle connection is envisioned that connects from Laurel Street to 6th Street aligning with 5th Street to break up the northern “mega block”.



On the southern mega-block, a street-like private drive and multi-use paths are envisioned to enhance connectivity and provide additional developable land.



RIVERFRONT NEIGHBORHOOD

This area is envisioned as transforming over time into a mixed use neighborhood oriented toward the river and park. Overarching principles include:

1) New development should engage and activate the river and park.

New development should consider uses that would help activate Two Rivers Park and the Colorado River. Appropriate uses include:

- Residential units (ownership or rental) would help activate the park. Higher end units could be achieved, given the proximity to the river.
- A restaurant, brewery, or similar food and beverage facility would also be appropriate. Summer river activity users could use a business like this to patronize during their stops.
- An indoor or outdoor recreation attraction could serve as another destination for this area that complements, rather than competes with, the existing destinations (park and hot springs).

2) New development should preserve and enhance views.

New development should consider views to the river and downtown. Taller buildings would be more appropriate near the interstate with lower-scale buildings along the river's edge. Currently, buildings up to 5 stories, or 60 feet, are allowed by code.

3) Implement improvements to Centennial Drive and Devereux Road.

Iron Mountain Hot Springs has become a popular destination in the neighborhood, generating more traffic (both pedestrian and auto) in this area. The city should improve Centennial Drive and Devereux Road with sidewalks and on-street parking (on Centennial Drive). Devereux Road, due to the significant slope on either side, presents an opportunity to build up to the street on the north side (existing CDOT site and parking lot) with street-level retail while utilizing the grade change for below-grade structured parking.

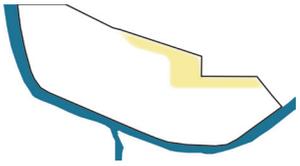
4) Future redevelopment of the CDOT property should continue to be explored.

The roughly 6 acre CDOT property at the intersection of Devereux Road and Centennial Street has been discussed as a major opportunity for redevelopment. The current location is not ideal for CDOT, as it is difficult for maintenance vehicles to access the interstate. If CDOT offices relocated, it could free up the land for a higher and better use that fits the vision for this neighborhood. The visibility from I-70 provides a prime real estate opportunity and amenities nearby like the park, river, and hot springs is unprecedented in Glenwood Springs.

5) Implement a riverwalk.

A riverwalk is envisioned along the Colorado River from Iron Mountain Hot Springs through Two Rivers Park with connections to the Confluence area and downtown, as well as to 6th street via the pedestrian tunnel. New development should plan for these improvements and encourage access to the river, both private and public, where feasible.



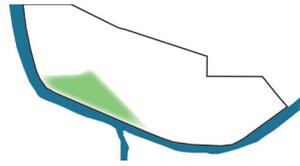


NORTH GLENWOOD RESIDENTIAL NEIGHBORHOOD

This is a neighborhood of mostly single family homes interspersed with some multi-family units. The streets are generally very narrow, some lacking sidewalks which forces pedestrians, bicyclists, and automobiles to share the same space. The historic street and building character is envisioned as remaining. New development within proximity to this character area should take into consideration the desire to keep this neighborhood residential in nature, with quiet streets and a dark night sky.



New development within proximity to this character area should take into consideration the desire to keep this neighborhood residential.



TWO RIVERS PARK

Two Rivers Park is included as a separate character area, as it is a public amenity and regional draw. The park is highly used and could benefit from more parking (or shared parking with new development) for events. Improving the ramps for kayakers and rafters is desired, as well as making the park a destination for rafters to stop and picnic or hang out. The southern edge is envisioned as becoming a beach where people can interact with the river and a boardwalk is envisioned along the river along Two Rivers Park and over to Iron Mountain Hot Springs.



The southern edge is envisioned as becoming a beach where people can interact with the river.

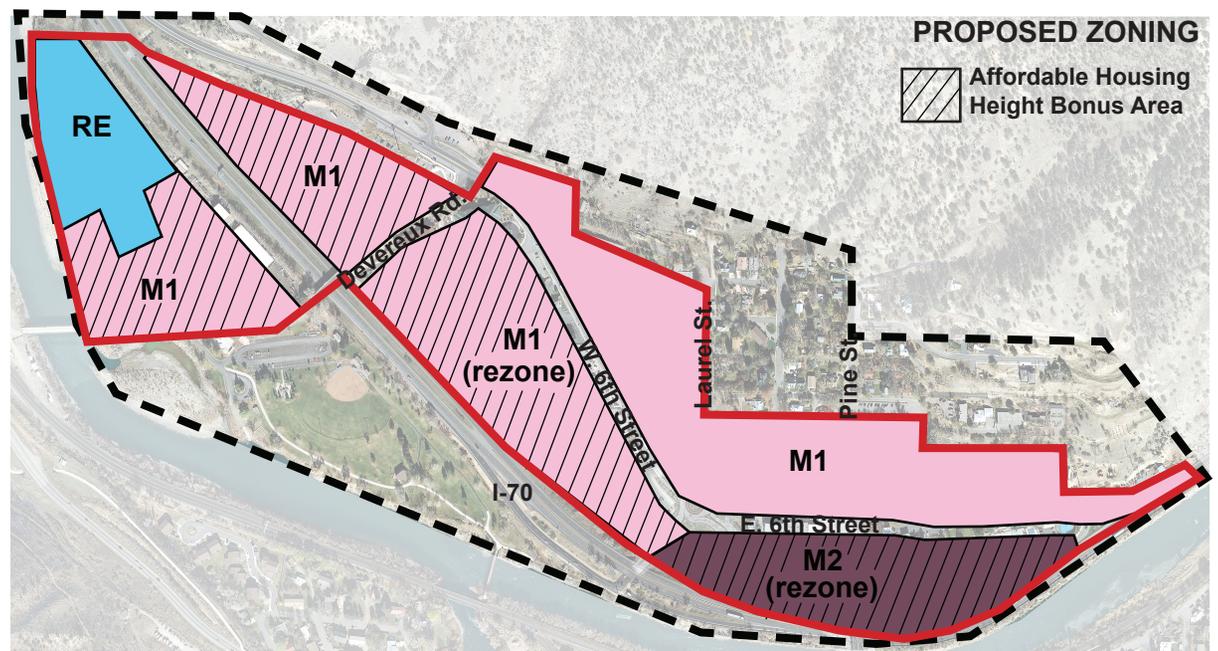
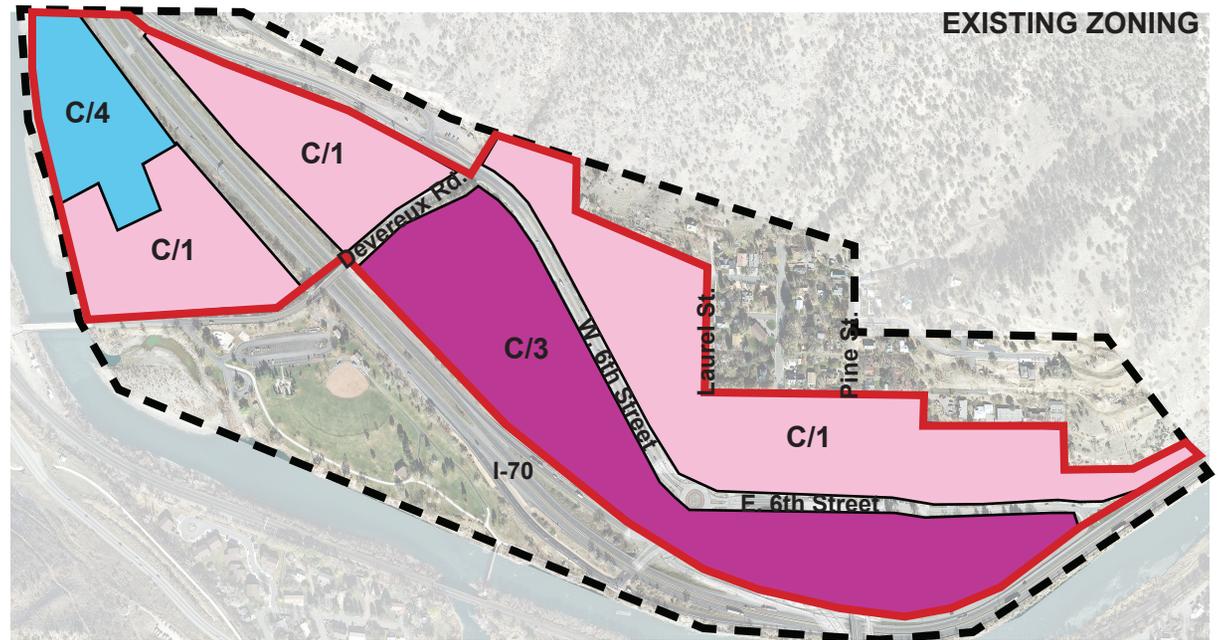


Improving the ramps for kayakers and rafters is desired, as well as making the park a destination for rafters to stop and picnic or hang out.

ZONING RECOMMENDATIONS

The Glenwood Springs Development Code Update was drafted concurrently with this plan. The new code is expected to be adopted in Winter 2017/18, following adoption of this plan. New regulations related to zoning will then be in place, along with the design standards and guidelines from this plan (Appendix A). A series of “replacement” zones are established which modify nomenclature and standards to better represent the existing zones’ intents. Portions of the 6th Street Corridor study area align with the proposed “replacement” zones, while other portions are recommended to be remapped to zones that better fit the desired character. The figures to the right show the existing and proposed zoning.

- The area **north of E. 6th Street**, currently zoned C/1, within the applicable boundary (i.e. excluding residential and public park zones) will be **replaced with M1** when the new code is adopted. The new M1 standards align with the vision of this plan and therefore no map changes are recommended.
- The properties **south of E. 6th Street and east of Laurel Street** and the new roundabout are currently zoned C/3. The replacement zone (CO) does not meet the future vision for this area, and therefore this plan recommends **rezoning to M2** - the same zone as downtown.
- Properties **south of W. 6th Street**, between Laurel and Devereux Road, are currently zoned C/3. The recommendation is for this area to be **rezoned to M1**.
- The area within the **Riverfront Neighborhood character area** currently includes two zones - C/4 and C/1. This plan recommends the **replacement zones of RE and M1** remain in place.



Listed below are the purposes and standards for each zone district, as outlined in the proposed Glenwood Springs Development Code Module 2:

M1 - MIXED USE CORRIDOR

Purpose - The M1 district is intended to accommodate mixed-use development with walkable and active streetscapes on Glenwood’s primary roadways. The mix of uses in the M1 district is intended to accommodate lighter-intensity commercial districts with limited auto-oriented uses compatible with surrounding residential neighborhoods. The M1 district is also intended to offer a diverse mix of housing opportunities at varying densities.

M2 - MIXED USE CORE

Purpose - The M2 district is intended to accommodate walkable and active development within Glenwood’s core commercial areas, including but not limited to central downtown. The M2 district includes the most recognizable and prominent areas of Glenwood Springs and therefore places an emphasis on high-quality building and site design, as well as the surrounding streetscape. The M2 district is intended to allow for a mix of residential and nonresidential uses while ensuring compatibility with surrounding established neighborhoods.

RE - RESORT

Purpose - The RE district is intended to accommodate high-intensity commercial development with flexibility in terms of allowable uses and dimensional standards to accommodate for primary and accessory resort development uses. The RE district is intended to apply to properties that are relatively isolated from adjacent properties due to physical and/or natural features.

The proposed standards for each district, as identified in Module 2 Public Draft of the Code, are as follows. Recommended amendments to meet the goals of the 6th Street Corridor Plan are identified in red.

	M1	M2	RE
Lot Standards, Minimum			
<i>Lot area</i>	<i>None</i>	<i>None</i>	<i>None</i>
<i>Landscaped area</i>	<i>15 percent</i>	<i>None</i>	<i>10 percent</i>
Setbacks			
<i>Front, minimum</i>	<i>5 feet</i>	<i>None</i>	<i>5 feet</i>
<i>Front, maximum (arterial streets)</i>	<i>60 feet</i>	<i>40 feet</i>	<i>60 feet</i>
<i>Front, maximum (other streets)</i>	<i>25 feet</i>	<i>20 feet</i>	
<i>Side, minimum</i>	<i>5 feet</i>	<i>None</i>	<i>None</i>
<i>Rear, with alley, minimum</i>	<i>5 feet</i>	<i>None</i>	
<i>Rear, without alley, minimum</i>	<i>7.5 feet</i>		
Building Standards, Maximum			
<i>Building height, Grand Avenue and E. 6th Street</i>	<i>--</i>	<i>40 feet</i>	<i>-</i>
<i>Building height, Other streets</i>	<i>*38 feet</i>	<i>50 feet</i>	<i>60 feet</i>
<i>Building height for meeting affordable housing incentive in 6th Street Corridor Plan (applicable areas)</i>	<i>60 feet</i>	<i>50 feet</i>	<i>--</i>

* - This plan recommends a 38-foot base height limit for M1 to allow a 3-story mixed use building of adequate floor heights and quality.

AFFORDABLE HOUSING HEIGHT INCENTIVE

The city should consider an incentive program that encourages affordable housing (rental and for-sale) by allowing additional building height (see above table) in the targeted areas within the 6th Street Corridor Plan study area (see hatched area of the proposed zoning diagram on the previous page). Details of defining affordability thresholds and duration should be worked out by the city. The city can build on the recent ordinance that allow fee waivers in exchange for providing affordable units.

NOTE:
 Refer to Appendix A for additional design standards and guidelines for applicable properties. Also reference the Glenwood Springs Development Code (anticipated adoption Winter 2017/18) for allowable uses and other dimensional and general development standards.

4. Mobility

6TH STREET CORRIDOR PLAN

MOBILITY GOALS

Currently, the primary function of 6th Street is to move cars. As currently designed, it is an inhospitable environment for pedestrians and bicyclists. Some overarching goals for improving mobility in the corridor study area include:

- To promote Glenwood Springs as an **outdoor recreation destination**.
- To **facilitate walking and biking** as viable transportation options by implementing comfortable and easy-to-use facilities.
- To **connect the major activity centers** within Glenwood Springs to one another.
- To **connect the two major regional trails** - Glenwood Canyon Trail and the Rio Grande Trail.
- To provide better circulation within the study area by **breaking up the mega-blocks** along West 6th Street with pathways, streets, or private drives.
- To supplement walking, biking and driving with an **efficient transit system**.
- To support redevelopment by implementing a **public parking structure** and other parking mechanisms.

The following pages outline a vision for enhancing mobility options and connectivity within the study area.

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PROPOSED BRIDGES	35
PEDESTRIAN AND BIKE FACILITIES	37
TRANSIT SERVICE	39
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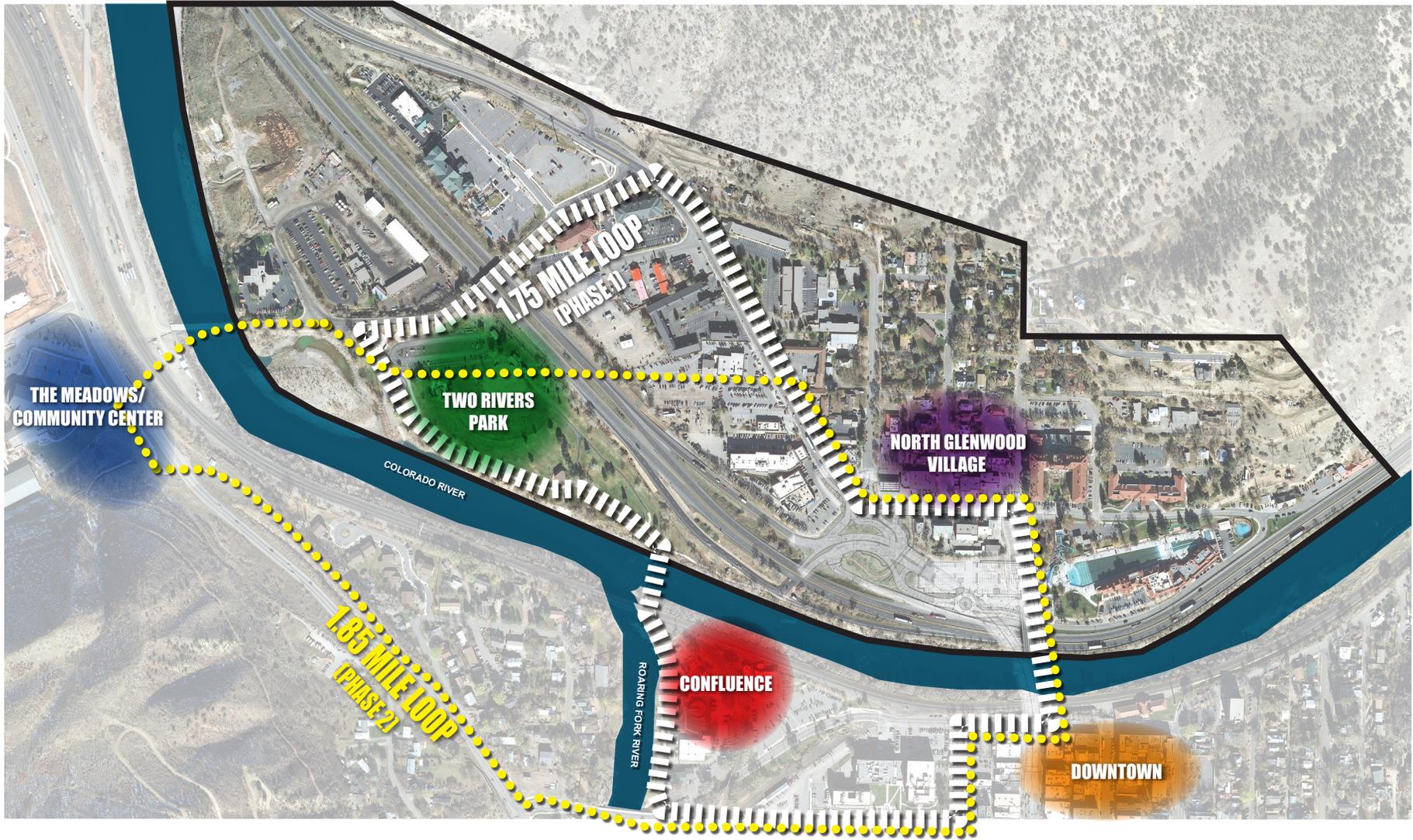


Figure 7 - The Glenwood Loop

THE GLENWOOD RIVERFRONT LOOP

In order to promote Glenwood Springs as an outdoor recreation destination and seamlessly connect the major activity centers, this Plan recommends implementing a bike/pedestrian “loop” system.

The proposed Glenwood Loop offers two phases:

- **Phase 1** - 1.75-mile hiking/biking loop connecting downtown, North Glenwood Village Core, West 6th Street corridor, Two Rivers Park, and the Confluence. This trail would include specific wayfinding signage to direct people along the route. It could also include kiosks with historical information about certain areas along the route, as well as awareness about health benefits such as estimated calories burned and distance information.
- **Phase 2** - 1.85-mile loop stretches over to the Glenwood Meadows shopping center and Midland Avenue. This extension is dependent on a new bridge connecting Devereux Road to Midland Avenue. A new pedestrian bridge across I-70 is also envisioned for this phase.

This hiking and biking trail could be promoted as both heritage and recreational tourism and include “smart technology” applications for handheld devices.

Phase 1 of the Glenwood Riverfront Loop utilizes existing trails and bridges, where feasible. In addition to new vehicular and pedestrian bridges, new improvements needed to implement the first phase include:

- **New bicycle and pedestrian facilities** along the north side of 6th Street from Pine Street to the Devereux Road. Refer to Chapter 5 and Appendix B for more information regarding streetscape design.
- **A pedestrian/bike crossing** at Devereux Road and West 6th Street.
- **A continuous multi-use path** along the south side of Devereux Road from West 6th to Centennial Street.

Phase two improvements include:

- **A signalized pedestrian crossing** on West 6th Street, approximately 350 feet west of the new Laurel Street interchange.
- **A new multi-use path** that stretches from West 6th Street to I-70 (approximately in line with 5th Street).
- **A pedestrian bridge over I-70** to connect the loop trail into Two Rivers Park.
- **A new vehicular bridge** to connect Devereux Road to Midland Avenue.

Note that for both phases, the **connection through The Confluence** depends on how that area redevelops. The trail is shown connecting via 8th Street to Colorado Avenue to 7th Street to get people back to the Grand Avenue pedestrian bridge.



The Glenwood Riverfront Loop could be “branded” and marketed in a variety of ways.



A smartphone app could be downloaded and include directions, historical information, health information, and more.



Public art and wayfinding could be incorporated along the route.

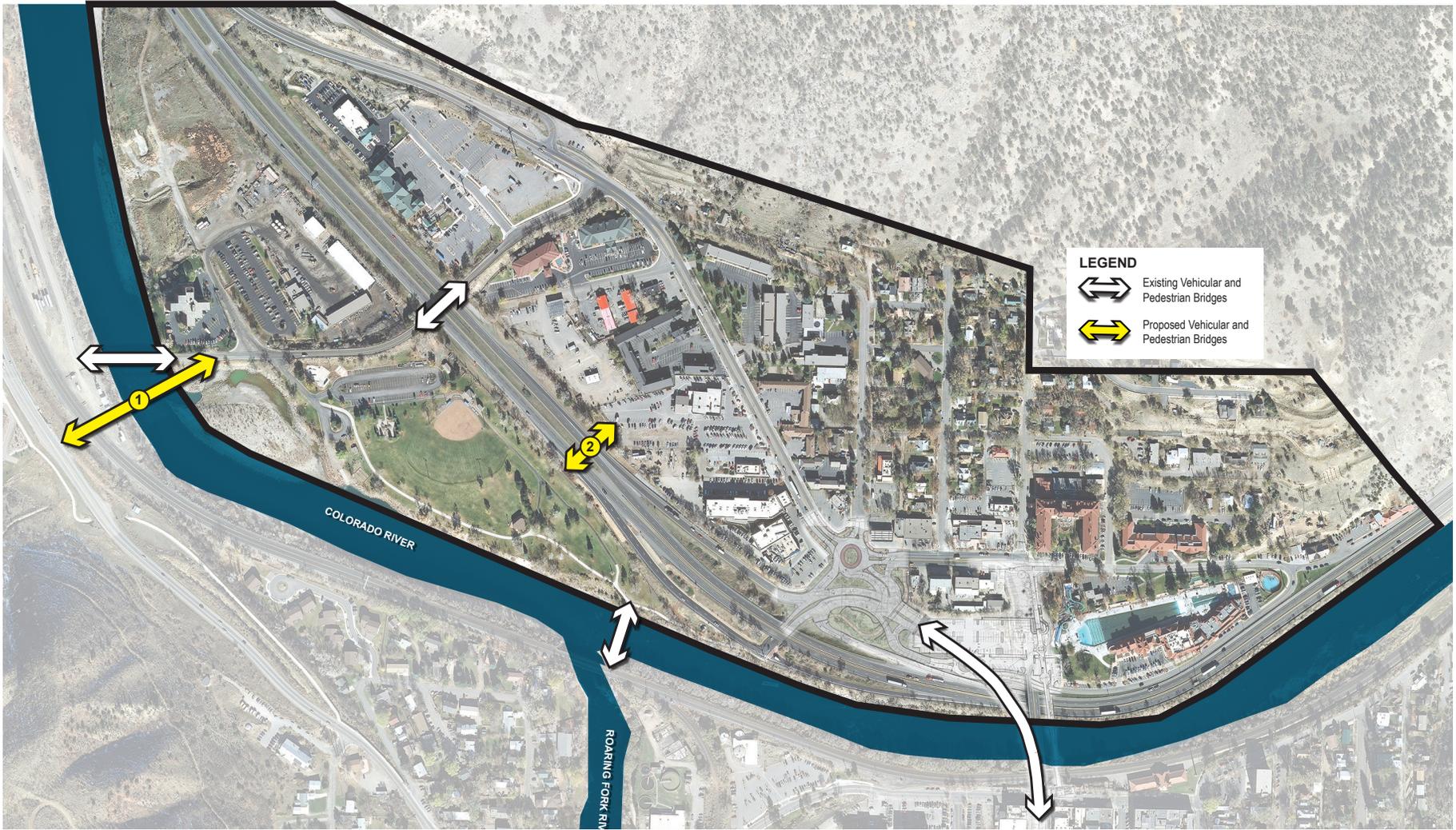


Figure 8 - Proposed Bridges

PROPOSED BRIDGES

To facilitate increased connectivity to and from North Glenwood, one pedestrian bridge and one auto bridge are desired:

#1 - DEVEREUX-MIDLAND BRIDGE

The proposed Devereux-Midland Bridge connects North Glenwood to Glenwood Meadows commercial area. The existing Devereux Road vehicular bridge over the Colorado River would remain in place to serve the south riverfront area. The new bridge would cross the railroad tracks and land at-grade with Midland Avenue near the Community Center.

This connection has been desired for many years. With new improvements and development opportunity in North Glenwood, the connection could become more feasible. The design of the bridge should include gateway features such as special structural treatment, lighting, banners, and more. It is also important that this structure not create an “under the bridge” condition where the existing kayak/raft pull-out and future beach area is to be located.

#2 - I-70 PEDESTRIAN BRIDGE

This bridge would cross I-70 midway between the Devereux Road pedestrian bridge and the new interchange. It is envisioned as becoming an extension of Two Rivers Park and connect into a new “civic spine” - a street/plaza that links directly into 6th Street. This “spine” would include a mixture of uses to activate it. It would also be connected to a public parking structure and transit center, also envisioned for this area (explained later in this chapter in more detail.)

This bridge should become an iconic gateway from the interstate. It should be designed as a seamless connector between the park and North Glenwood and feel more like a grand plaza than a bridge. It could include lookout areas to view downtown and the river.



The “civic spine” concept seamlessly connects Two Rivers Park to North Glenwood.



The I-70 Pedestrian Bridge should be designed more like a grand plaza than a bridge.



The Glenwood Canyon Trail Bridge would provide additional connections to the trail and contribute to the larger Glenwood Loop concept.

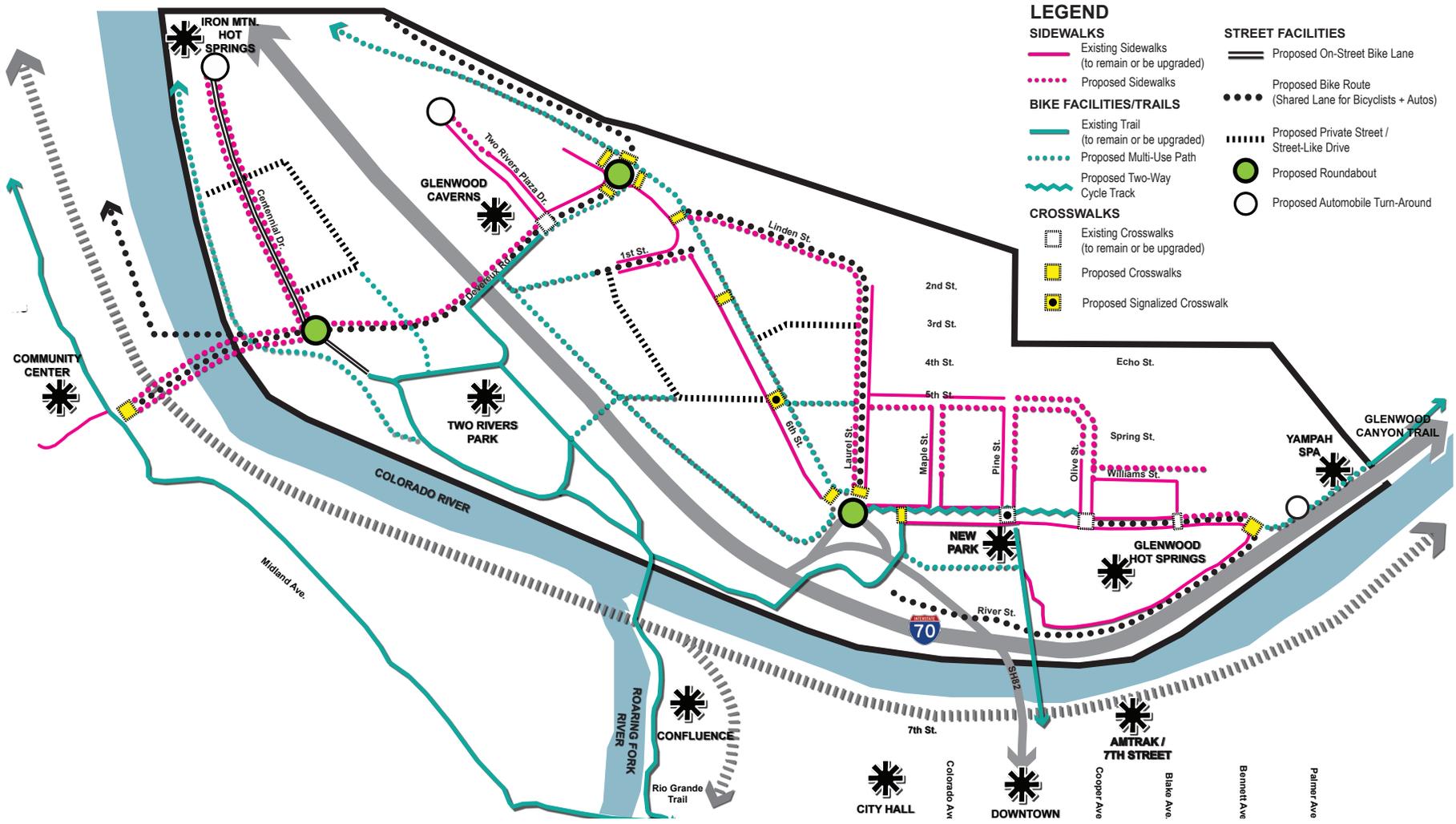


Figure 9 - Pedestrian & Bike Connectivity

NOTE:
 Refer to detailed street sections in Appendix B for detailed diagrams of sidewalk, bike lane, and multi-use path recommendations.

PEDESTRIAN AND BIKE FACILITIES

Pedestrian and bike connectivity is a priority for North Glenwood and the 6th Street Corridor study area. Figure 9 shows a variety of proposed solutions for improving circulation and connectivity in this area. Explanations are as follows:

SIDEWALKS

Figure 9 shows the location of existing and proposed sidewalks (solid and dotted pink lines, respectively). Implementing a continuous network that is accessible and offers a pleasant and safe experience is important. The following guidelines relate to new sidewalks:

- Sidewalks at minimum, should be 5 feet wide, except along 6th Street where sidewalks should be a minimum of 6 feet wide (clear space). Note that the current code requires 8 feet in commercial areas.
- Where feasible, sidewalks should be buffered from the street with tree lawns, landscaping, or trees in grates.
- All streets within the “Village Core” area (between Laurel and Olive Streets from 5th to 6th Streets) should include sidewalks on both sides of the street.
- Devereux Road from Centennial Drive to Two Rivers Plaza Drive, although narrow and constrained by topography on both sides, should include a sidewalk on at least one side of the street.

CROSSWALKS

Crosswalks are essential to promoting walkability by providing multiple safe route options as a pedestrian. Crosswalks should be clearly marked so pedestrian areas are highly visible to oncoming traffic. In some locations, pedestrian signals may be desired, such as Rectangular Rapid Flashing Beacons (RRFB). In other locations, the material change and pedestrian crossing sign may be enough. And still in others, especially long-term, a full traffic signal may be appropriate. Further traffic studies will be needed to determine the need once the new interchange is in place.

Guidelines for new crosswalks are as follows:

- New crosswalks should be accented with a change in material. Laid brick pavers that match the ones specified in chapter 5 for 6th Street streetscape palette are preferred. If not feasible due to cost or maintenance, a colored and stamped concrete in brick herringbone pattern is preferred.
- All materials and colors for crosswalks should match the overall streetscape palette.
- Crosswalks should be at least 8 feet wide.
- Accessible ramps should be provided at the sidewalk edge.

Refer to Chapter 5 for more information on crosswalk locations.



Sidewalks should be buffered from the street to provide a safe and comfortable pedestrian experience.



Crosswalks should be accented with a change in material such as pavers or colored, stamped concrete.



The same pavers used on 7th Street downtown are the same pavers specified for 6th Street. Crosswalks should match these in color or material.

TWO-WAY PROTECTED BIKE LANE

A two-way protected bike lane is proposed along the north side of E. 6th Street from Olive to Laurel. This facility is separated, or “protected”, from automobiles by being raised at curb level and buffered with bollards. Using contrasting paver colors and patterns can visually separate the bike lane from the sidewalk. They offer many benefits in a “main street” environment:

- They are **safer** for bicyclists - reducing the number of conflicts with moving autos and parked cars opening doors (a very common bicycle conflict).
- They are **family-friendly** and even safe for small children to use.
- They contribute to **placemaking** - or making a place feel special and unique.
- They **boost economic growth!** A recent report from PeopleForBikes and Alliance for Biking & Walking suggests through various case studies that protected bike lanes have: fueled redevelopment to boost real estate values, helped companies attract workers who wish to bike to and from work, encourage workers and residents to be healthier and more productive by providing active transportation options, and increased retail visibility and sales by placing more PEOPLE closer to businesses as opposed to CARS.

MULTI-USE PATHS

Multi-use paths (MUPs) are dedicated off-street paths that are wider than sidewalks and that accommodate both pedestrians and bicyclists in the same space. During development of this plan, a voting preference exercise with the public revealed that the majority of residents preferred off-street facilities to on-street facilities (bike lanes or routes) in this area.

Some MUPs exist (Two Rivers Park, Midland Avenue, and Glenwood Canyon Trail) and new MUPs should be incorporated in key areas that connect into the existing network. Refer to Figure 9 for key locations.

The following guidelines for MUPs should be observed:

- Multi-use paths should be at least 10 feet wide. Twelve feet is preferred.
- Maintenance is essential to the use of MUPs. The City should consider snow and rock clearing and other general maintenance for these facilities.
- Lighting and signage should also be considered to ensure that the MUPs are safe and inviting at all times of day.

BIKE LANES AND ROUTES

Because MUPs are not feasible to implement everywhere due to land constraints, some on-street facilities are proposed to provide the “missing links” in the overall trail system.

This plan proposes on-street bike lanes where streets are wide enough, or could be reconstructed to accommodate them within the existing rights-of-way. Where land is more constrained, a shared facility will suffice. These are indicated with “sharrows” in the travel lane to warn automobiles to share the lane with bicyclists.



Multi-use paths are wider than sidewalks and accommodate both pedestrians and bicyclists in the same space.



Protected bike lanes provide a dedicated space for bicyclists that is buffered from cars and pedestrians.



Sharrows warn automobiles to share the lane with bicyclists.

TRANSIT SERVICE

EXISTING TRANSIT SERVICE

Ride Glenwood buses are operated by the City through a contract with RFTA and service the 6th Street corridor at 30-minute headways from 6am to 7pm. This route serves Glenwood Meadows, West Glenwood, North Glenwood, downtown and up-valley to the Roaring Fork Market Place. There are two stops within the study area along West 6th Street, but the route misses some of the major destinations such as Glenwood Hot Springs and Iron Mountain Hot Springs.

Other RFTA service includes **Local Valley Bus** (service to Aspen), **Hogback** (service to Rifle), and the **VelociRFTA** (service to Aspen). Another option is the CDOT **Bustang** (service to Denver). However, all of these buses pick up across the river from 6th Street at the West Glenwood Park & Ride. The **Colorado Mountain Express** (service to Denver International Airport) picks up at the Ramada.

PROPOSED SHUTTLE LOOP & FUTURE TRANSIT

A more localized shuttle is desired for the 6th Street corridor study area to take visitors to and from major destinations (see Figure 10). A short-term route is proposed that would begin at Iron Mountain Hot Springs and end downtown, servicing all major destinations along 6th Street in between. When the Devereux-Midland bridge is constructed, a longer route could be implemented that also services The Meadows and connects into the West Glenwood Park & Ride.

The proposed shuttle should be visibly different from Ride Glenwood to avoid route confusion and to “brand” itself as a unique transit system.

As Glenwood Springs matures, more transportation options such as car sharing and ride sharing should

Chapter 4: Mobility

also be accommodated through dedicated on-street parking spaces or reserved spaces in future parking structures.

TRANSIT STOPS

The stops for the shuttles should incorporate the unique branding of the vehicles as well as signage indicating time tables, maps, and other necessary information. They should be designed as “mini plazas” where their use is flexible, blends in with the streetscape and supports interaction.

Shelters are desired to protect transit riders from the elements. The architecture of the shelters should be a blending of artistic expression and the traditional architectural character of North Glenwood. Along 6th Street, a dedicated area for buses/shuttles to pull out of the traffic lanes is desired, however this will depend on the location and right-of-way that exists.

The following guidelines for transit stops should be followed:

- Where feasible, a dedicated “pull-out” for buses should be provided so traffic can pass by.
- Consult RFTA guidelines for dimensional standards for bus pull-outs. Shuttle-only stops will depend on the size of vehicle chosen.
- Place transit shelters directly adjacent to the property line with a 6-foot minimum sidewalk in between the shelter and the curb/loading zone.
- Transit shelters should have a minimum footprint of 60 square feet and be at least 4 feet wide.
- Transit shelters are preferred at each stop, but should at least be provided every other stop.
- Where shelters are not provided, at least one bench, trash receptacle and appropriate signage should be provided.
- Traditional materials such as brick, wood, glass and metal are encouraged. Durability is important.
- Shelters should be as transparent as possible to deter negative behavior.



Ride Glenwood and the shuttle service would share stops along West 6th Street.



A pull-out for buses is desired. A 6-foot minimum sidewalk between the shelter and the curb should be provided.



The proposed shuttles should be visibly different from Ride Glenwood to avoid route confusion and to “brand” itself as a unique transit system.

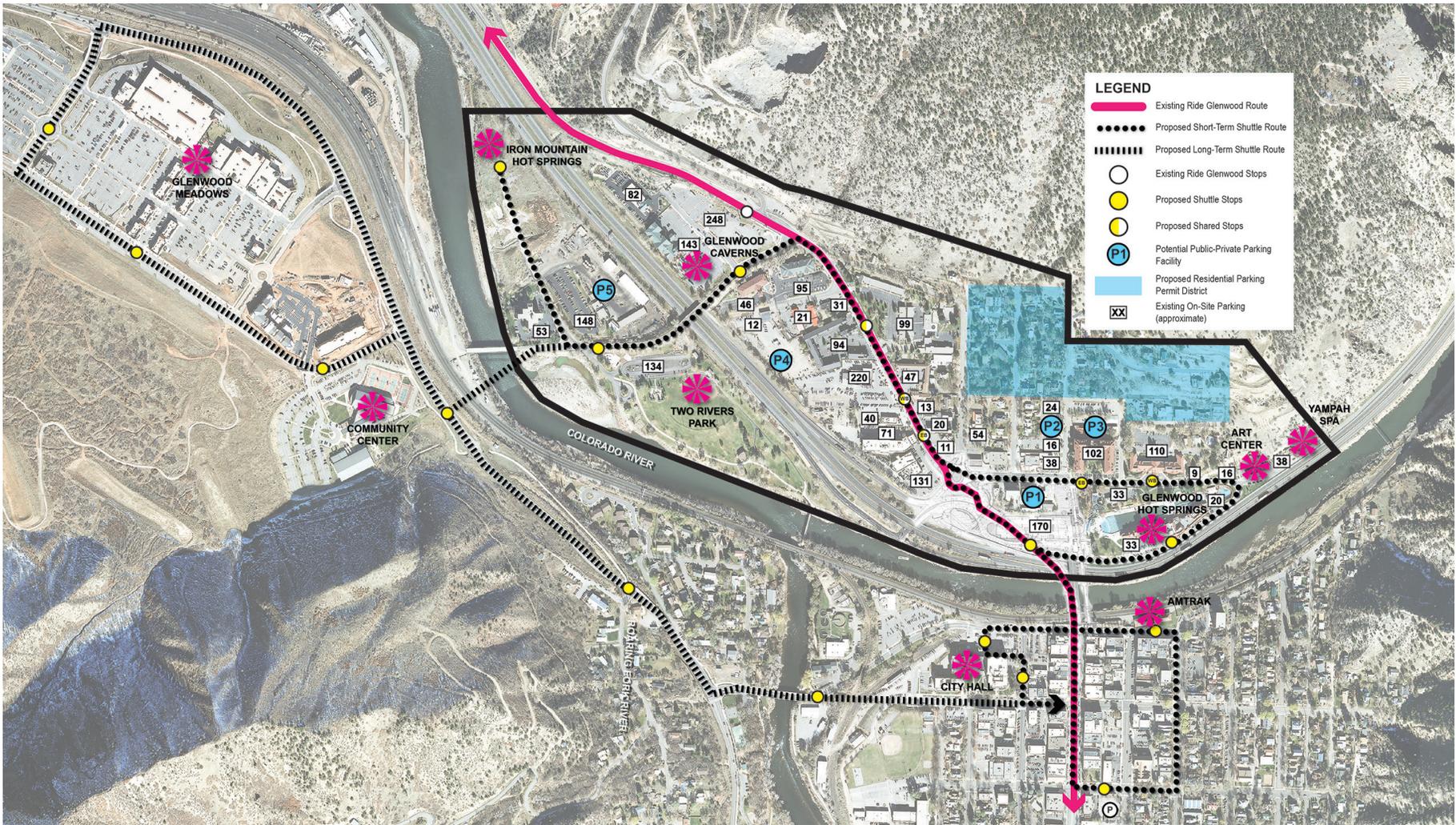


Figure 10 - Transit and Parking

PARKING

Currently, the study area lacks sufficient parking to serve existing uses, especially during the busy tourist season which is generally May through September. This is due to the small lot sizes, older buildings and infrastructure, and topography paired with existing zoning standards and lack of public parking. Providing public parking to serve multiple businesses and lessen the burden on local property owners and residents is desired. Providing public parking spaces would also free up additional developable land. Some suggested actions are listed below:

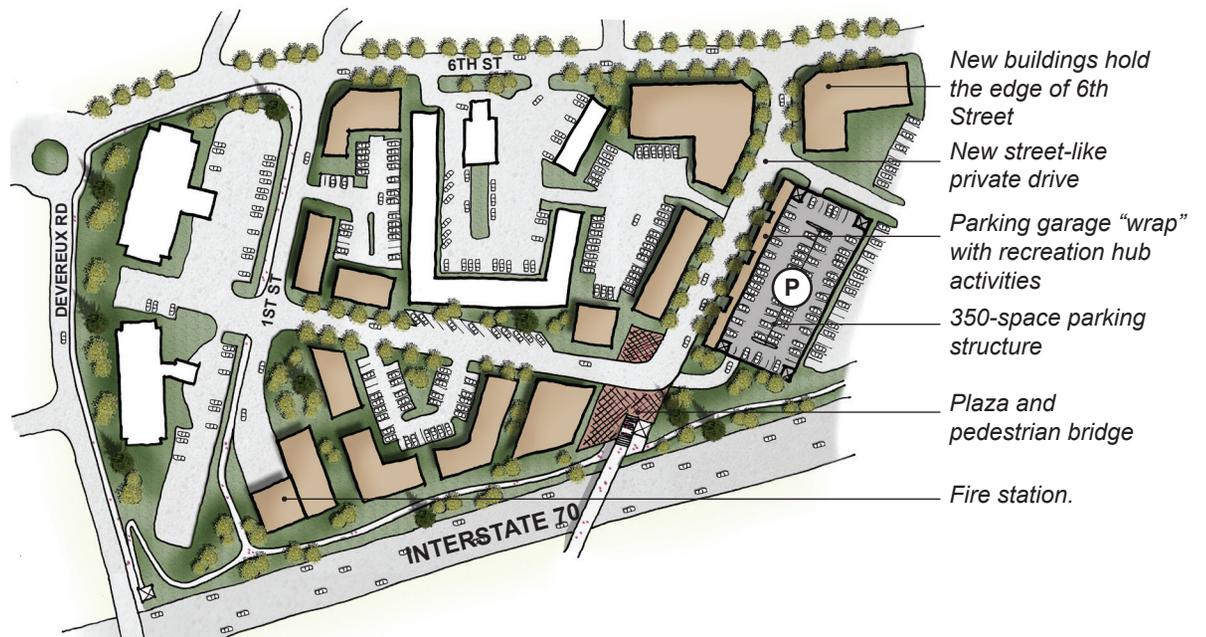
IMPLEMENT A PUBLIC PARKING STRUCTURE

A public parking structure (or structures) is a priority for supporting future redevelopment in the area, as well as to relieve existing parking pressure. Several potential locations are identified in Figure 10. This does not infer that five parking structures are feasible in this area, especially in the near term. Rather, they are shown as desired potential locations based on criteria such as: proximity to downtown and local destinations, service area, future uses, and available land.

All of the potential parking locations are shown on private land. This means that the City would either need to purchase the land in order to build a structure or partner with the existing land owner to jointly develop the structure. A joint partnership may be desired by property owners as a way to help fund their own redevelopment vision.

In particular, a structure labeled P4 in Figure 10 was considered a good location for a public parking structure, due to its proximity to and visibility from the interstate. This parking structure is envisioned as serving multiple purposes, including:

- **Public parking** to serve the 6th Street corridor as well as overflow parking for Two Rivers Park events.
- **A Park & Ride** for North Glenwood, which could be served by Ride Glenwood as well as regional transit services such as RFTA, CDOT's Bustang and Colorado Mountain Express.
- **Overflow parking for Hanging Lake** - with the current parking lot for Hanging Lake over capacity, a satellite lot with a shuttle is needed. This parking structure could be an ideal location for Hanging Lake parking spillover and transit service.
- **A Recreation Hub** - the parking structure could be "wrapped" with a Recreation Hub facility. Inside, kiosks from local tourist attractions could be available to provide a "one stop shop" for tourists to book activities. A climbing wall or other indoor (or outdoor) recreation facility could provide activities while people wait.
- **A Fire Station** - relocating the downtown fire station to North Glenwood has been discussed, as the downtown location is not ideal for these services. A fire station could be incorporated into the parking structure as well to provide an active civic use for the area. A North Glenwood location gives the fire department better immediate access to the interstate than the current 8th Street location. It would also help improve the department's response times to emergencies in Glenwood Canyon.



A conceptual plan rendering shows how a new public parking structure could be incorporated into a larger development and serve the entire 6th Street corridor, offering a "park once" strategy.

RESIDENTIAL PARKING PERMIT DISTRICT

In order to protect the residential neighborhood from spillover parking, the City should create a Residential Parking Permit District where parking would only be allowed for cars with designated stickers for certain hours. Currently, this system is being implemented in some neighborhoods near downtown. The proposed area is shaded in Figure 10.

IN LIEU FEES FOR PARKING

The City should consider allowing “in lieu fees”. These fees are paid by developers to the city when a property redevelops. The revenue is used by the city to create public parking spaces. Benefits include:

- **Flexibility** - Relieves developers from having to provide on-site parking thus giving them the opportunity to utilize valuable land area for building.
- **Shared Parking** - in lieu fees allow the city to take into consideration the peak parking demands of the various uses in the area. For example, an office and a bar would have different parking demands as they are used at different times.
- **Park Once** - providing shared, public parking in a single location encourages users to park once to visit multiple businesses, thus alleviating traffic congestion.
- **Better Urban Design** - parking requirements often result in surface parking lots servicing small buildings that cannot afford structured parking. By combining parking for multiple uses into one facility, it allows more developable land (and perhaps more sales tax revenue) to be created and fewer parking lots, which contributes more positively to a walkable, sustainable, and thriving district.

PAID PARKING

In the long-term, the City should consider creating a parking district to collect funds for parking as a source of revenue that could be used for maintenance and future parking growth. Although it might not seem feasible at the onset, as demand for parking grows, paid parking may become feasible.

Paid parking should start with on-street spaces, as there is a premium for convenience parking. This could occur along East 6th Street and along certain north-south streets between 5th and 6th. Paid parking in public structures could be feasible for special events in the short-term, and could expand into the high-season with free parking allowed when the demand is lower.

Paid parking would require a more detailed study. However, the City should be aware of the potential revenue generation for the future.

PARKING STRUCTURE DESIGN

The following general guidelines should be followed for parking structures (Refer to Appendix A for additional guidance):

- Utilize topography to mask the scale of the structure. In some locations (P1, P2, P3), there is opportunity for a two-level parking structure, with no internal ramps, which is less expensive.
- Design parking structures to be similar in mass, scale, style, and materials to existing, surrounding buildings.
- Parking structures should be extra sensitive in mass, scale, and architectural character if they abut residential properties. For instance, using a finer grain of detail and smaller scale is appropriate.
- Wrap parking structures with commercial uses whenever feasible. This will help activate the street and positively contribute to the community.



Parking structures should utilize the topography to mask the scale of the overall structure, like this parking structure in Rifle, Colorado.



Parking structures should be architecturally similar to their surroundings and “wrapped” with commercial uses whenever feasible.



A parking structure could be wrapped with unique architectural treatment and activities such as a rock climbing like “The Block” in Chattanooga, TN.

5. Streetscapes, Public Art & Signage

6TH STREET CORRIDOR PLAN

STREETSCAPE GOALS

The existing streetscape design for 6th Street was implemented in the 1980s and is outdated. The future vision for 6th Street is to become a destination for both residents and visitors by activating the street edge with desired uses such as shopping, dining and small public gathering spaces. The new streetscape should present a pedestrian-friendly, safe and welcoming experience with improved beautification including landscaping, lighting, public art, and other amenities that make walking pleasant and effortless. Traffic-calming features such as on-street parking, narrower drive lane widths, and landscaping will slow down cars. Bicycling will also be promoted with proper facilities to encourage the use of bicycles as a viable mode of transportation.

DEVOLUTION FROM CDOT

The entirety of 6th Street west of Pine Street is currently owned and maintained by CDOT and is categorized as a state highway. However, with the new I-70 interchange configuration, 6th Street from Pine to Laurel streets will be transferred to City of Glenwood Springs ownership in a process referred to as “devolution”. In the transfer, CDOT provides the City with a one-time payment to cover the costs of roadway maintenance over a 20-year period. The payment does not have to be earmarked for maintenance. In addition to devolving 6th Street between Pine and Laurel, this plan recommends devolution of Highway 6 from Laurel to Traver Trail.

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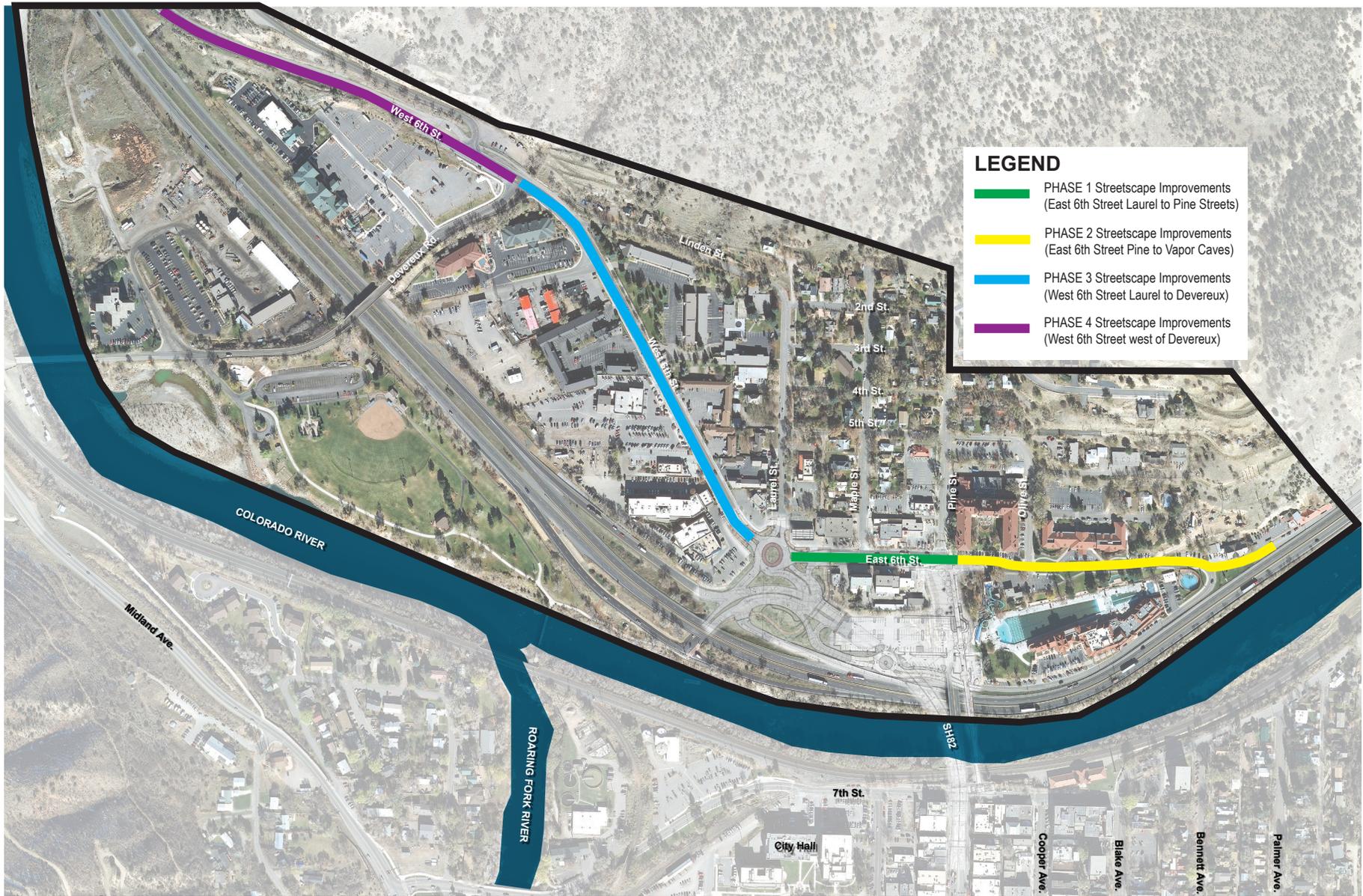


Figure 11 - Streetscape Improvements - Phased Map and Legend

STREETSCAPE PHASING

Major streetscape improvements are proposed for the entire 6th Street corridor from the Vapor Caves to West Glenwood. The following explain the anticipated phasing of improvements for 6th Street:

PHASE 1 IMPROVEMENTS

Phase 1 includes improvements to East 6th Street from Laurel Street to Pine Street (refer to Figure 11), including

- complete re-build of the street right-of-way, due to a decrease in traffic volumes once the new interchange is complete.

These improvements should be completed immediately following the construction of the new roundabout and Grand Avenue Bridge. Anticipated completion of this phase is by the end of 2018 (refer to Chapter 7 for more information).

PHASE 2 IMPROVEMENTS

Phase 2 includes East 6th Street from Pine Street to the Vapor Caves. This includes:

- continuing the same street configuration as Phase 1 in front of Hotel Colorado to Olive Street;
- minor improvements between Olive and River Drive to provide continuous sidewalks;
- intersection improvements at 6th and River Drive;
- moving the Glenwood Canyon Trailhead up to the intersection;
- new multi-use path along I-70 from the new trailhead to the regional trail;
- improvements to Yampah Spa parking area; and
- improved shared street/plaza in front of the Art Center.

These improvements should be completed immediately following Phase 1 improvements. Anticipated completion of this phase is by 2020. It is important to note that it will not be possible to undertake these improvements if this section of west 6th Street remains a state highway.

PHASE 3 IMPROVEMENTS

Phase 3 streetscape improvements includes West 6th Street from Laurel Street to Devereux Road, including:

- reconfiguration of right-of-way to provide a wide, multi-use path, or promenade” on the north side, narrower travel lanes, a wide sidewalk on the south side, new crosswalks, and the addition of street trees and landscaping, signage/wayfinding, transit stops, and other placemaking features.
- A roundabout is proposed at the intersection of West 6th and Devereux Road to better organize circulation in this area, making it safer and more efficient. It also serves as a significant gateway into the corridor from West Glenwood.

These improvements will enhance the character of the village core and should be implemented as soon as possible, but may only be feasible once the City takes over ownership and maintenance of the street from CDOT. Anticipated completion of this phase is by 2022.

PHASE 4 IMPROVEMENTS

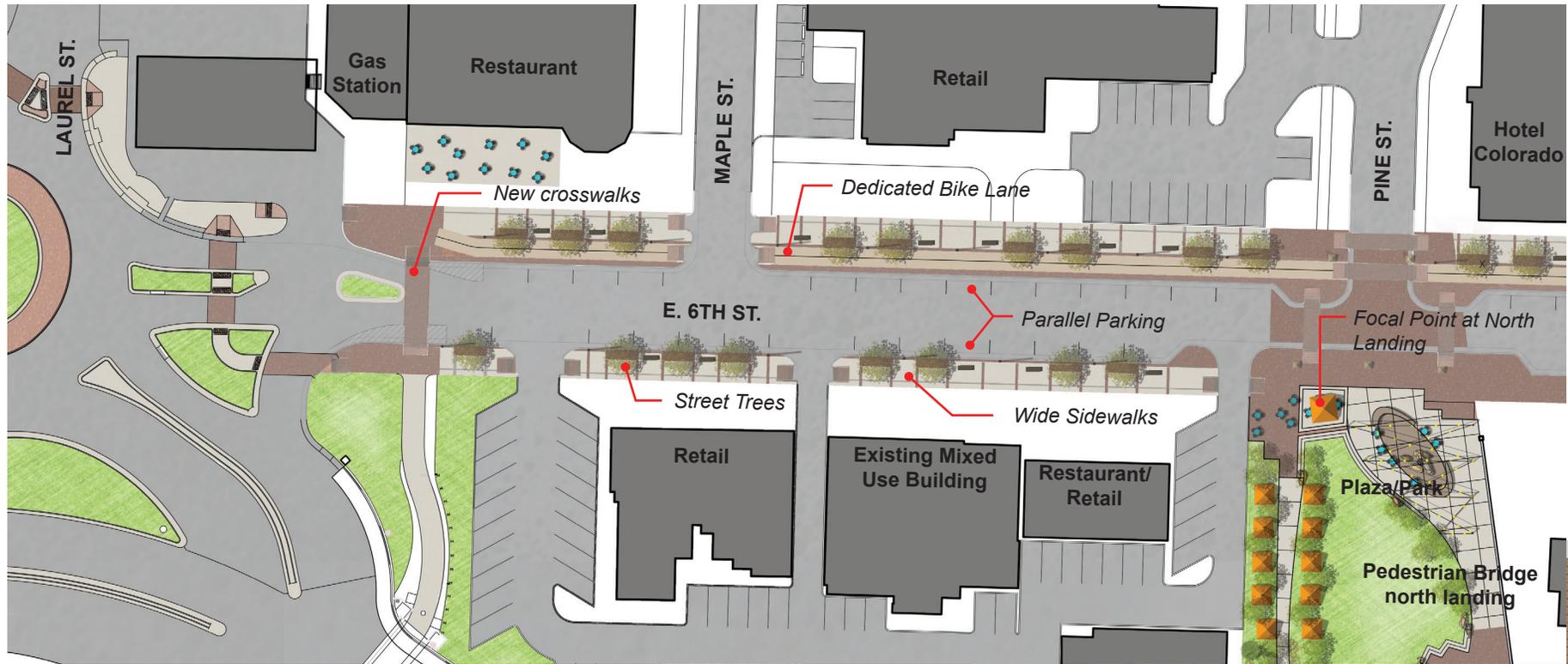
Phase 4 consists of implementing pedestrian and bike facility improvements to Highway 6 west of Devereux Road. Currently, a multi-use path exists along the north side of the road, but the pathway is not protected from the travel way and is often covered in gravel, making it unsafe to use. The proposed street section adds a curb and gutter with a landscaped median and 12-foot wide multi-use path.

These improvements will serve as a cohesive connection between North Glenwood and West Glenwood and make bicycling between the two areas a viable alternative. These enhancements should be pursued following the devolution of the street from CDOT. If improvements must be phased, this section should be last.

NOTE:

Street sections for all streets mentioned (phase 1-4) are provided in Appendix B. Please reference these for recommended improvements.

Phase 1 - East 6th Street between Pine and Laurel Streets:

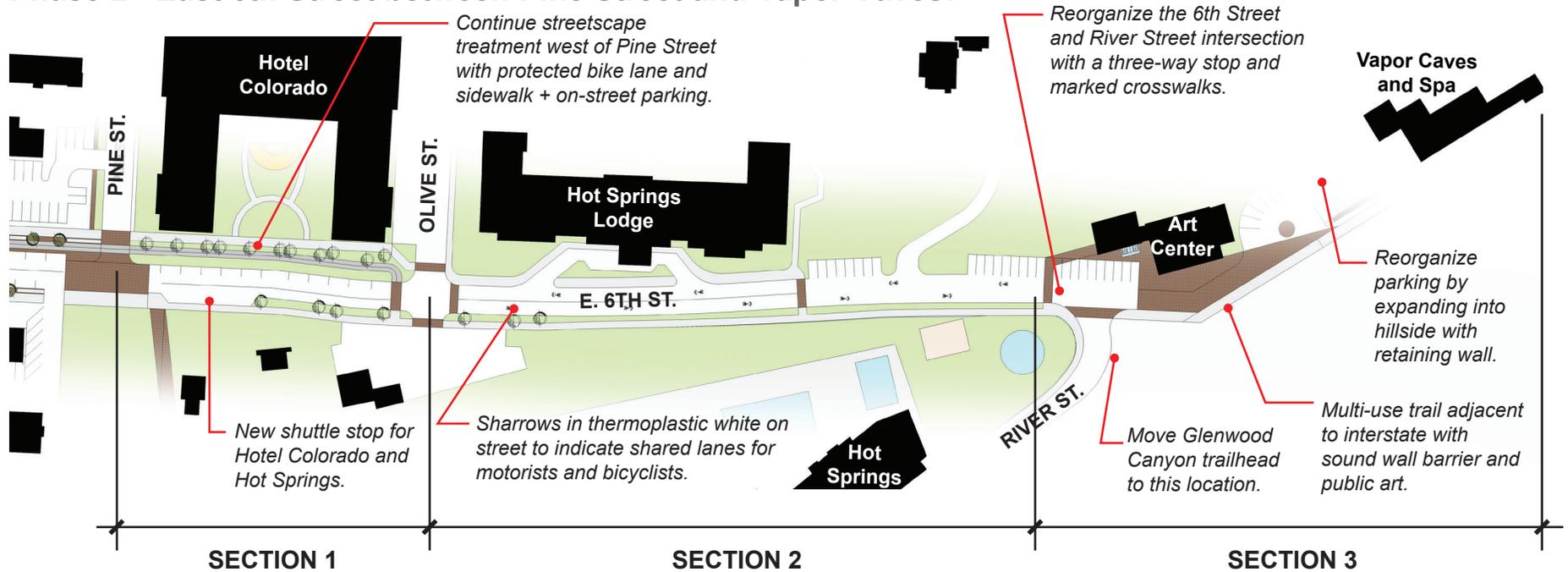


Plan Rendering for E. 6th Street from Pine to Laurel.



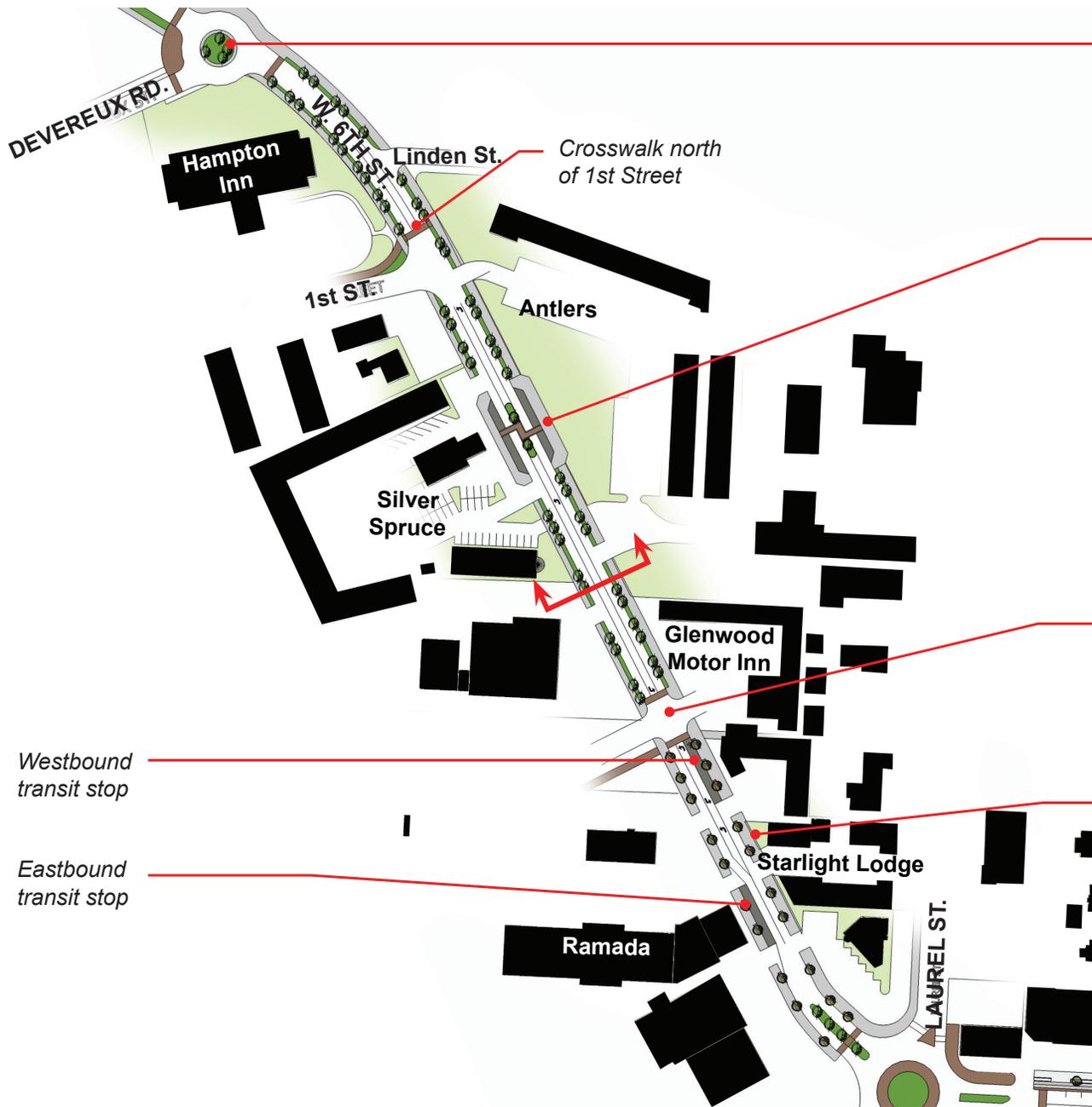
Perspective looking east from crosswalk in front of gas station. New development shown on south side of street is for illustrative purposes only - it is not a development proposal.

Phase 2 - East 6th Street between Pine Street and Vapor Caves:



Perspective looking east from 6th and North River streets illustrating relocated Glenwood Canyon trailhead, and new I-70 gateway entrance to the city with enhanced landscaping and public art.

Phase 3 - West 6th Street between Laurel Street and Devereux Road



New roundabout and gateway feature; move Traver Trail to intersect at roundabout.

Crosswalk north of 1st Street

Transit stop at Silver Spruce and Antlers (note: if public parking structure and transit hub is built, it would replace this stop).

Crosswalk and intersection at proposed 5th Street connection (future new street to west and pathway to east)

Westbound transit stop

Wider multi-use path (north side only)

Eastbound transit stop

NOTE:
Execution of streetscape improvements on W. 6th dependent upon devolution of Highway 6.

STREETSCAPE MATERIALS

Sidewalk improvements along 6th Street should include a higher degree of detail, as opposed to other streets in the study area. This is to highlight and unify the 6th Street corridor. The materials should be timeless and easy to maintain overtime.

The majority of the streetscape consists of standard gray concrete with a brush finish. In certain areas, a more detailed scoring pattern is proposed to further delineate space and function within the streetscape. Brick paver accent banding is consistent throughout the corridor from Pine Street to Devereux Road. This material is specified to use the same brick pavers from 7th Street in order to tie the two streets together aesthetically. The 6th Street pattern purposefully mimics the 7th Street pattern, but is the inverse (i.e. 6th Street uses brick pavers as the accent as opposed to 7th Street which uses brick as the primary surface material). A buff colored concrete is proposed for the protected bike lane on the north side of 6th Street from Laurel to Olive. The color differentiation will provide a visual contrast so that the bike lanes are not confused with the sidewalk. A bike symbol should also be painted onto the surface for further clarification.



The 6th Street pattern purposefully mimics the 7th Street pattern (above) but is the inverse.



Standard Gray Concrete with a Brush Finish



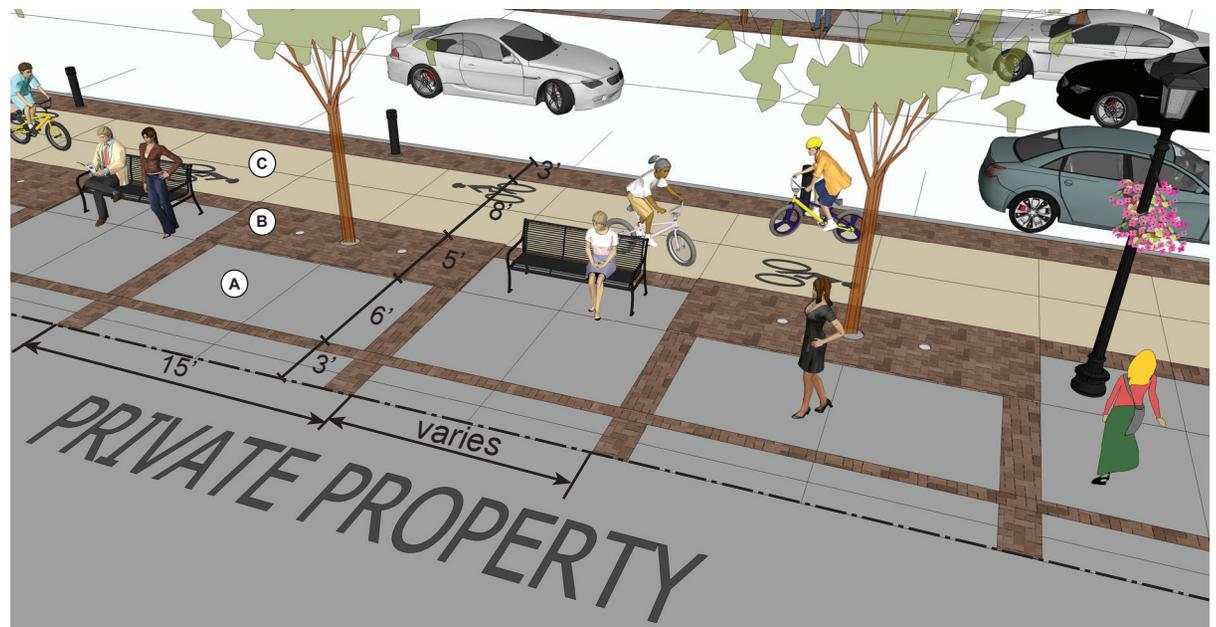
Endicott - Medium Ironspot No. 46 brick pavers (4x8) in herringbone pattern



Colored Concrete - Mesa Buff (Davis Colors)



West 6th Street paving pattern. Note the change at the future intersection in front of Rosi's where street trees transition from paver grates to tree lawns.



East 6th Street paving pattern

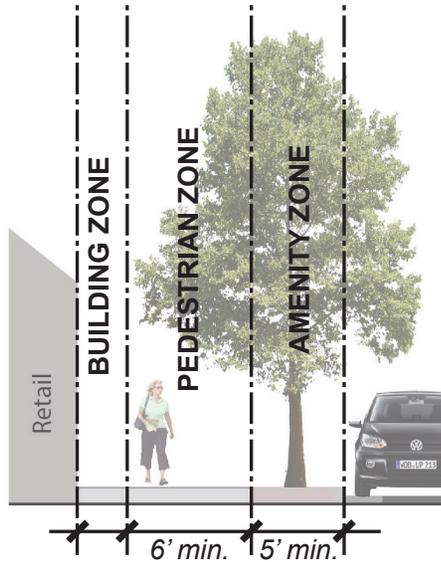
STREETSCAPE KIT OF PARTS

The streetscape “kit of parts” will tie the corridor together aesthetically by using black, powder-coated metal as the consistent color/material. The matte black color also contrasts well with the horizontal material palette. Each of these elements should be placed within the “amenity zone” of the sidewalk as shown in the illustration below.

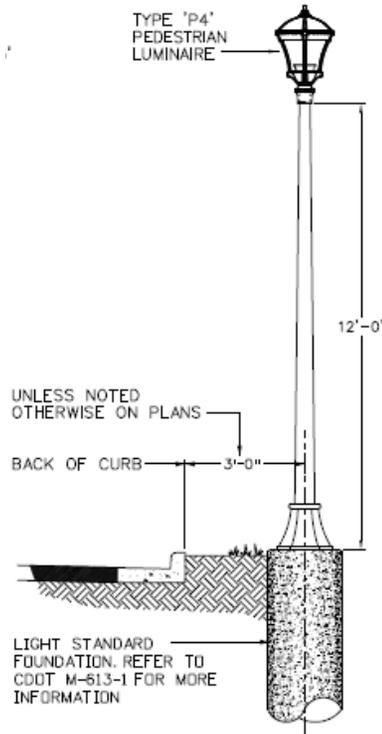
LIGHTING

New pedestrian lighting should match the light fixtures on the new Grand Avenue pedestrian bridge (see excerpt at right from lighting package). Bollards are also planned to further delineate space and to add ambient lighting, especially at the intersection of Pine and 6th and along the protected bike lane.

Pedestrian lights should be spaced approximately 40-feet apart, but no more than 75-feet.



6th Street streetscape area should be split into three “zones” whenever feasible.



The pedestrian lighting will be the same as is being used on Grand Ave. pedestrian bridge.



Architectural Area Lighting - Providence Medium LED lamp



LandscapeForms “Annapolis” Bollard - matte black powder coated metal

FURNISHINGS

Benches and trash receptacles should utilize existing city specifications for consistency and efficiency. Bike racks should be simple, sleek, timeless, functional, and vandal-proof.

Within the Village Core, two benches per block face is appropriate (i.e. eight benches between Pine and Laurel). Outside the Village Core, benches should be spaced a minimum of 75 feet apart. Provide one trash receptacle per block face, or a minimum of 300 feet apart. Bike racks should be strategically located to be convenient for bike traffic.



Landscape Forms Chase Park 40 gallon trash receptacle in black powder coated steel with modified bear resistant top (or other similar bear resistant style).



Landscape Forms 35 Loop Bike Rack



Landscape Forms Plainwell Bench with lpe wood and black metal.

LANDSCAPING

A consistent landscaping treatment should be applied throughout the corridor. Street trees are desired to “soften” the streetscape and provide shade in the summer months. Another consistent landscaping treatment is the use of annuals in hanging baskets from the new pedestrian light fixtures, a feature designed to give 6th Street corridor a unique character. This landscaping treatment will be unique to North Glenwood and help contribute to its identity.

STREET TREES

Street trees should be spaced approximately 30 to 35 feet apart and shall be planted with a 3-inch caliper. There are two conditions for planting street trees along the corridor:

- **Paver Grates** - this system includes a 5' x 15' tree pit with a suspended metal grate for placing brick pavers upon. This system allows water to permeate through the pavers into the soil and provides ample air for healthy tree growth. This system should be applied in the Village Core area.
- **Tree Lawn** - this system includes a 5' min. tree lawn. The lawn area could include sod or low native grasses and shrubs. This condition applies to everywhere except for the Village Core.

Preferred street tree species include Honeylocust variety, Littleleaf Linden, Japanese Lilacs and non-fruit bearing trees like Cleveland Select or Callery Pear. Others may be used as accents or for special circumstances.

HANGING BASKETS

Hanging flower baskets are to be applied to the lighting fixtures throughout the 6th Street corridor. A flexible fixture that allows hanging baskets or banners to be applied should be considered so that when flowers are not appropriate, banners could be hung instead.



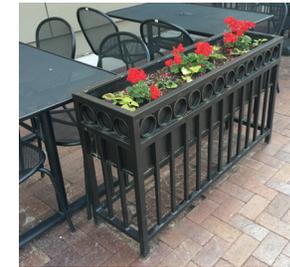
Hanging flower baskets are to be applied to the lighting fixtures throughout the 6th Street corridor.



The Honeylocust variety is currently present on the corridor and is preferred for new street trees.

OTHER LANDSCAPING

Private properties are encouraged to contribute to the landscaping of 6th Street with planters or other moveable landscape treatments. These elements should be placed in the “building zone” and would be owned and maintained by the business or property owners. They should be similar in scale, color, material, and style as the proposed streetscape kit of parts.



Businesses on 7th Street have implemented planters that also serve as enclosures for cafe seating.



Planters adjacent to storefronts contribute to the landscaping of 6th Street.



Planters would be owned and maintained by the business. Planters should be similar in scale, materiality, and aesthetics as the proposed streetscape kit of parts.

PUBLIC ART

Public art contributes to a unique identity and is encouraged throughout the 6th Street corridor study area. Art pieces should be strategically placed for high visibility. They may be located along the street, mixed in with other streetscape elements, or in dedicated areas such as parks and plazas. They may be traditional, functional, or whimsical - but they should be scaled appropriately for the environment in which they are situated. For example, larger exhibits are more appropriate in wide-open spaces such as parks whereas smaller, more human-scaled exhibits are more appropriate within the streetscape and adjacent to buildings.

A new public gathering place proposed at the intersection of 6th and Pine, adjacent to the new pedestrian bridge, is intended to feature public sculpture. A rotating sculpture exhibit is proposed for the southwest corner of the park in an "art walk." These pieces will be displayed on stone slabs so that when there is not an art piece present, the stone slab may serve as a bench. Additionally, a large, prominent, focal sculpture is proposed in the public gathering space near 6th Street. The sculpture will serve a dual purpose - as public art and a unique climbing feature for children.

The City should work with the Art Center and commission local artists whenever feasible for permanent sculptures or exhibits. The art walk is intended to be a dedicated space for local artists as well. Other, temporary art exhibits may also be considered for seasonal draw or special occasions.



A teddy bear sculpture is planned as a focal point for the 6th Street Park as an art piece as well as a play area for kids.



Public art may be incorporated into streetscape elements such as benches.



An art walk is proposed for the southwest corner of the park. Sculptures will be placed on large slabs of stone. When art pieces are not present, they will serve as additional seating areas.



This play on street signs is located in front of the Aspen Art Museum.



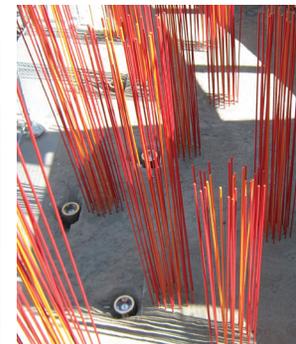
A temporary "pink balls" exhibit occurred in Montreal with pink balls strung on lines and hung over the street.



Public art can be whimsical.



Larger exhibits are appropriate in parks.



Colorful exhibits are eye catching.

SIGNAGE

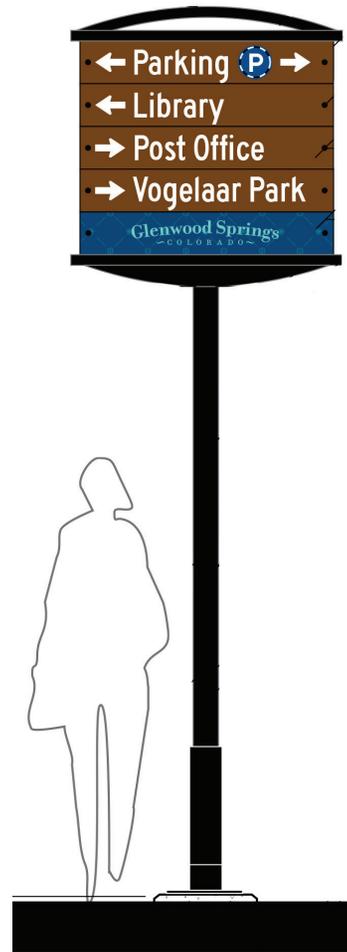
Figure 12 calls out general placement and messaging for new wayfinding signage for the 6th Street corridor. Signage should follow the City's existing Wayfinding Sign Program Standards Manual. There are seven sign types identified for the corridor:

- Type A1 - Primary Vehicular Directional
- Type B2 - Secondary Vehicular Directional
- Type F1 - Parking Lot Directional
- MUTCD - Vehicular Safety/Regulatory
- Type G - Pedestrian Directional
- Type H - Pedestrian Directory
- Type E1 - Park Identification Sign

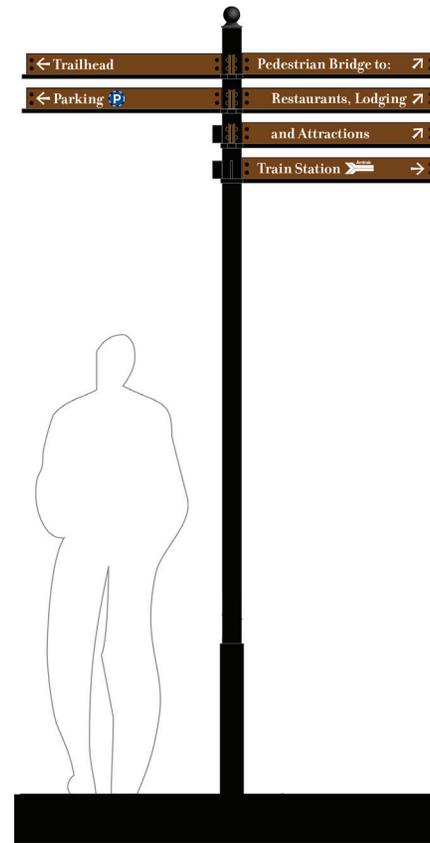
Types A1, B2, MUTCD, and G include preliminary messaging for the signs. Other types include general placement. Further development of a signage program for the corridor will be needed following adoption of this plan.



The backs of signs have a custom graphic that make them appealing to look at, even from behind, and call out to passersby that there is messaging that might be of interest



Example of sign type A1 - Primary Vehicular Directional



Example of sign type G - Pedestrian Directional



Example of sign type H - Pedestrian Directory

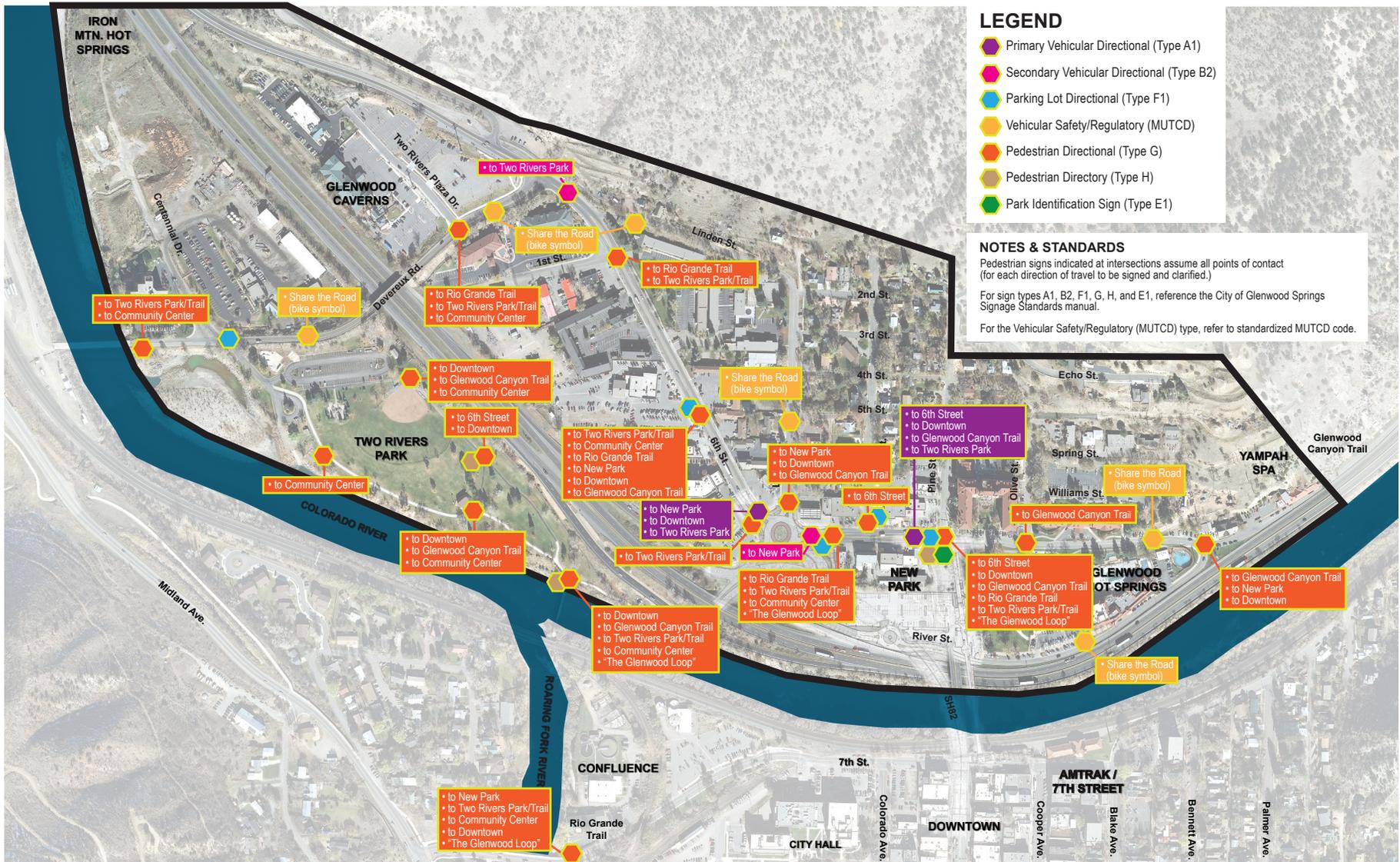


Figure 12 - Wayfinding and Messaging

6. Parks & Open Space

6TH STREET CORRIDOR PLAN

PARKS AND OPEN SPACE GOALS

Currently, **Two Rivers Park**, the City's largest regional park, is the only public park in the study area. It includes a band shell, baseball field, boat ramp, paved multipurpose trail, skate park, sculpture memorial, and public restrooms. One of its greatest amenities is its half-mile of frontage along the Colorado River. The park is highly active in summer months with boating activities, outdoor concerts and special events, but it could use improvements to restore the river bank and to add enhancements that better connect the park and its users to the river.

Other open space within the study area, includes a few, large landscaped areas that are located on private property. These spaces are good examples of utilizing private property to provide semi-public gathering areas:

- **Antlers Best Western** has a large, green space along 6th Street. This park-like setting includes a grassy area with shade trees and a large bronze sculpture of a moose in center as a focal point..
- **Hotel Colorado** at Pine and 6th includes a large courtyard that serves as a semi-public space. The courtyard is landscaped and includes pathways from the street to the hotel, a veranda and an outdoor cafe.. This space is especially vibrant during the holidays when the hotel and trees are adorned with festive lights and people gather to watch the annual fireworks display.

Primary goals for improving parks and open space in the study area include: implementing a new public park or gathering place at the intersection of Pine and 6th Streets adjacent to the new pedestrian bridge; improving Two Rivers Park to better engage the river; and encouraging private property owners to create landscaped areas around their properties.

Chapter 6: Parks & Open Space

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NEW 6TH STREET PARK OR PLAZA

LOCATION

Immediately adjacent to the new pedestrian bridge north landing is a 16,300 square foot area that was the former landing of the old vehicular Grand Avenue Bridge. A key focal point, this city-owned space can be converted, in whole or in part, to a public park or plaza that would provide a much needed gathering space to serve North Glenwood residents and visitors.

The new park will be at the crossroad of major pedestrian and bicycle routes which connect the 6th Street corridor with the Glenwood Canyon Trail, downtown and the Glenwood Loop, a new trail recommended in this plan. and future Glenwood Riverfront Loop, as well as to downtown Glenwood Springs via the new pedestrian bridge.

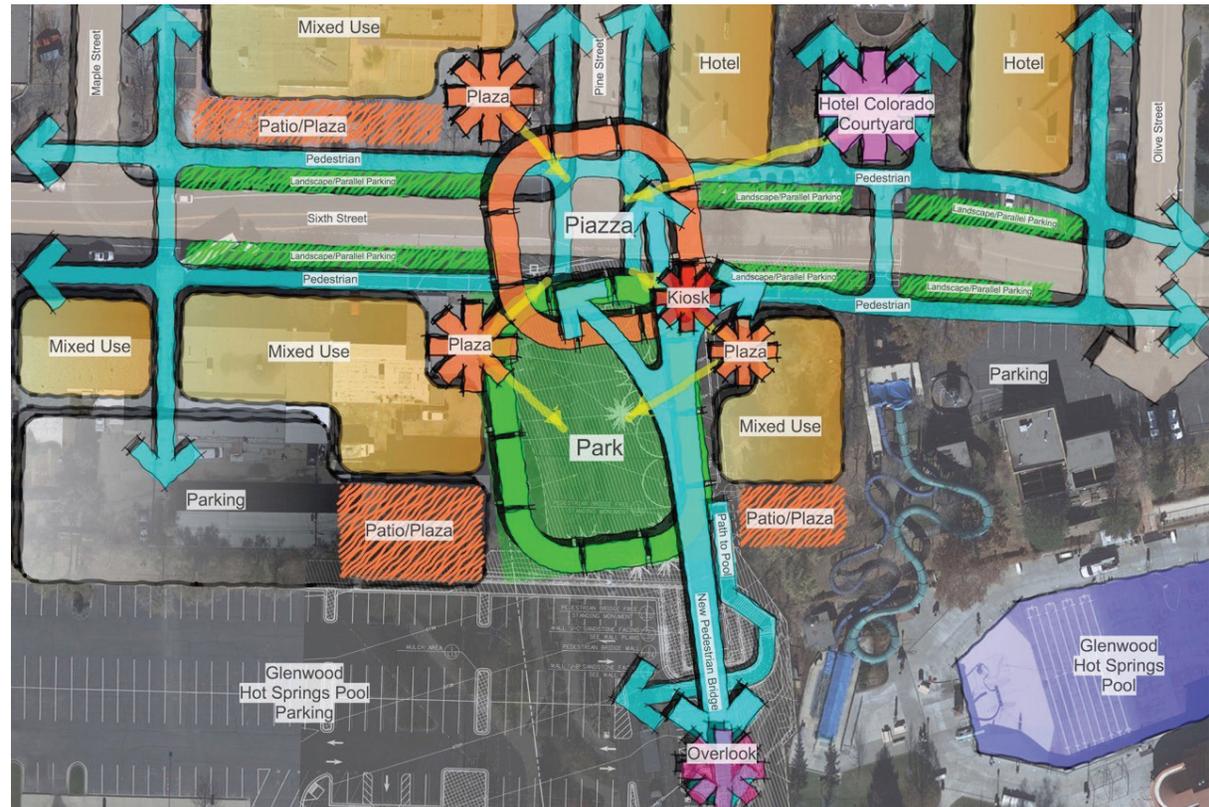


Figure 12 - Concept Diagram showing an option for a full-sized park covering the entire 16,300 square foot area.

CONCEPT

This park/plaza will serve as a focal point and gathering place for visitors, residents, and people coming to and from downtown. The pedestrian bridge will provide a seamless and comfortable pedestrian experience from downtown to North Glenwood.

The concept for the park/plaza includes the following main objectives:

- **Connect seamlessly with the pedestrian bridge.** There should not be a hard line where the bridge ends and the park/plaza begins. Rather common elements and materials will blend the two together.
- **Expand the park/plaza into the street with a “piazza.”** The intersection of 6th and Pine will integrate high-quality materials to accentuate it as a special place and to draw attention to the area. It could also be closed off for special events and truly serve as an extension of the park/plaza.

- **Activate the space!** Existing and future buildings surrounding the park or plaza should be designed to serve a mix of uses with active ground floors, such as coffee shops, cafes, restaurants or retail businesses and with upper floors devoted to housing. Small “plazas” at each of the corners of the 6th & Pine intersection are indicated on the diagram above. This simply means that when these areas redevelop, any future building should include active ground floor uses that spill outside to overlook and provide “eyes” on the park/plaza. This could include cafe seating, a small plaza with seating areas, or an expanded sidewalk area with outdoor retail or another type of active use. Housing on upper floors also would help add to the vibrancy and provide extra “eyes on the park”

- **Serve as a gathering place.** A large kiosk that includes a map of the area should be included in the park near 6th Street. It could also include advertisement space for nearby businesses. This space should serve as a place for people to meet up and make decisions about where they want to go. An iconic art piece or sign is also appropriate.



In the future, mixed use buildings with active ground floors, such as coffee shops, cafes, restaurants or retail businesses are encouraged to activate the park.



The intersection of Pine and 6th could be closed off for special events and serve as an extension of the park/plaza.



An iconic art piece or sign is appropriate to serve as a logical gathering, or “meet up” place. In Fort Collins, people gather around The Bear.



The bridge should blend into the park/plaza and the then into the street. New mixed use buildings should activate the park. Image above depicts a full-sized park option.

PROGRAMMING

For the park or plaza to be successful, it is essential that certain programming elements are included to keep the area active and vibrant throughout the year. This plan recommends:

- Organic shapes
- Informal children's play area
- Signature sculpture as "go to" element
- Open lawn area for special events or lounging
- Place for festival tents
- Native landscaping integrated into southern slope
- Landscaped buffer and low site wall on west property line
- Stage area for informal performances
- Overhead festival lighting
- Steps that serve as extra seating areas
- Hanging flower baskets on light poles
- Bistro tables and chairs
- Interpretive signs about Glenwood history
- Bike racks
- Bear resistant trash receptacles, dog bag stations
- Drinking fountain
- Electrical and water service for events and maintenance
- Public art and wayfinding signage
- Natural boulders and materials
- Subsurface stormwater drainage
- Interactive water feature



Steps that serve as extra seating.



Bistro tables and chairs.



Organic shapes.



Informal play area.



Stage area for informal performances.



Open lawn for special events or lounging.



Overhead festival lighting.



Natural boulders/materials.



Public art.

FULL PARK OPTION

Working closely with Parks Department staff, stakeholders and the community, this design option utilizes the entire 16,300 square foot area that was once the north landing of the Grand Avenue Bridge. This plan, pictured at right, shapes the space to become a special gathering place for North Glenwood residents and visitors. A description of the major park elements follows:

1. **6th & Pine Intersection:** this is a traditional street intersection with sidewalks, curb and gutter. It is designed to be a visual extension of the park and could be closed occasionally for special events.
2. **Park Plaza:** the north end of the park serves as a “gateway” to the park and a transition from the “Village Core” district. This area includes a kiosk for people to orient themselves and make decisions. It also includes streetscape elements such as pedestrian lights with hanging flower baskets, benches, and a combination of boulders and planters near the street edge.
3. **Informal Stage:** a slightly elevated space for informal performances is located in the northwest corner. It will also include bistro tables and chairs for socializing or having a picnic.
4. **Children’s Play Area:** the oval shape in the northeast corner of the park includes boulders and a unique sculpture for kids to play on. Bistro tables and chairs will surround the play area, as well as two low walls on either end of the oval that will serve as additional seating. This area also includes overhead festival lighting, integrated tensile shade structures, and an interactive water feature.
5. **Promenade:** the wide sidewalk and tree area on the west edge of the park serves as a place for tents to be set up during festivals or celebrations. Trees are spaced appropriately so that tents can fit in between them and electrical outlets will be incorporated into the low wall that defines the west edge of the park. The majority of the time, when tents are not present, these spaces will serve as additional leisure seating areas.

6. **The Lawn:** the lawn area serves as a flexible space for the park. It will be an area for people to hang out and relax in the grass. It will also be an area for events, both organized and informal, in the park such as: movie nights, yoga in the park, art classes, and more.
7. **Art Walk and Play Area:** the southwest corner of the park, at the terminus of the promenade, includes a “landing” area with seating and sculptures as well as an additional play area for children, which will help activate this area of the park. A pathway, or “art walk” wraps around the children’s play area which will include a rotating sculpture exhibit. Other children-related events like a story walk could also occur in this space.
8. **Landing:** this area is where the bridge “lands” and the park begins. This expanded space is intended to serve as a second “gateway” with a plaza with steps down to the lawn. It also is where the steps down to the Hot Springs Pool and parking area are located. A sign will be located here to direct pedestrians.
9. **Sloped Area:** this area slopes down to the retaining wall that CDOT will put in as part of the parking lot construction. It includes native seed planting and trees, as well as some large stone slabs that reinforce the “edge” of the park.
10. **Pedestrian Bridge:** the new pedestrian bridge will continue to downtown Glenwood Springs.



Conceptual plan rendering of park.

ALTERNATIVE STUDIES

Alternative design studies were explored due to the overwhelming positive reaction to the conceptual 3-story mixed use building originally shown as a “future” development scenario to activate the park on the western edge. Sparked by City Council’s curiosity, the design team explored alternative options for establishing an activating land use within the park/plaza boundaries on public property:

- **OPTION 1:** A 2-story mixed use corner building with ground floor retail and upper floor lofts is shown fronting onto the park and 6th Street. Behind that building, 4 townhomes are placed fronting the park to provide additional housing and “eyes on the park”. The design was modified to respond to the more formal edge with a mixture of hard lines and the original natural forms. Program elements remain, except for the informal stage on 6th Street. This option does not allow for on-site parking, as there is no room for access.
- **OPTION 2:** A 1-story retail kiosk building is located fronting the park/plaza and 6th Street. This allows for an active use within the space while still providing ample room for a number of amenities and active uses. Refinements were made to the design to respond to this alternative configuration.

NOTE:

The feasibility of these options will be dependent upon the ability to relocate a number of utility lines which criss-cross the property. This includes water, sewer, electric, and high-pressure gas.



Original park design with existing conditions shown to the west.



Original park design with future conditions showing the existing three buildings as redeveloping into a mixed use building. Note this is a conceptual proposal and did not involve input from the property owners.



Option 1 shows a 2-story mixed use building with townhomes fronting onto the park/plaza to activate and provide “eyes on the park.”



Option 2 shows a 1-story retail kiosk building to activate the space and provide ample amenities and activating uses. Refinements were made to the original, larger design to respond to this smaller alternative configuration.

TWO RIVERS PARK IMPROVEMENTS

Two Rivers Park is a wonderful amenity for North Glenwood, and the entire city. In the summer months, it is highly active with river sports activities and festivals and events in the park. However, there is a desire to better connect the park to the river and celebrate the Colorado River.

Concepts for enhancements to Two Rivers Park include:

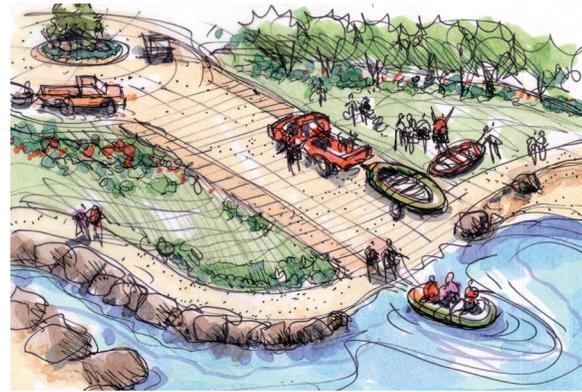
- An island **beach**
- Improved **boat ramp**
- A **riverwalk trail** along the river
- **Picnic areas** along the trail.
- In-stream **water park/surfing zone**
- Improved Two Rivers Park open space area for **special events** with new **pedestrian bridge** over I-70.

NOTE:

A separate design charrette including some members of the corridor plan design team assisted Parks and Recreation department with a visioning exercise to further develop concepts for Two Rivers Park. The sketches on this page are from that exercise.



Surfing zone and beach area.



Improved boat ramp.



Riverwalk trail and overlook.



Boulder seating area to watch kayakers and rafters.



Picnic areas along the trail.



Multi-use path connecting to Rio Grande Trail.

PRIVATE OPEN SPACE OPPORTUNITIES

Other, small open space opportunities on private property should be encouraged throughout the study area. Semi-public spaces on private property help activate and enliven the public realm especially if they occur in highly visible places such as along 6th Street.

A primary concept for enhancing the identity of 6th Street is for buildings to “open up” to the street and blend the outdoors with indoors. As properties redevelop, they should consider opportunities for blurring the line between public and private space. For example, as parcels redevelop, new buildings should be designed with large, traditional storefront windows and should include small entry plazas, courtyards, cafe seating areas, public art or other similar active spaces along the street edge.



Covered, outdoor gaming areas that can be seen from the street and sidewalk blur the line between public and private.



Cafe seating on both sides of the street provide unique experiences and activate the sidewalk.



Small seating areas or gathering spaces adjacent to a building are encouraged.



Small, semi-public plazas with landscaping and seating areas are a pleasant respite to a streetscape experience.



Opening up to the street with large garage doors and seating areas help activate the sidewalk and public realm.

7. Implementation

6TH STREET CORRIDOR PLAN

USING THIS PLAN

The City shall use this Plan as a way to carry out the vision for the 6th Street Corridor, as outlined in the past six chapters. Implementation measures include both administrative tasks as well as capital infrastructure projects. Funding sources and participation will be varied - from within the City, to quasi-governmental entities such as the Downtown Development Authority and/or a new authority, to private developers. Focusing on projects that can be leveraged through partnerships and synergies - where the priorities of this plan are met, as well as other city-wide priorities - is encouraged.

As stated in Chapter 1:

- **The city will use the plan as a way to prioritize public investment** in infrastructure such as streets, parking and utility upgrades, as well as placemaking elements like streetscapes, plazas and park improvements.
- **The city will also use the plan as a tool to evaluate future development proposals** of properties within the project area.
- **Landowners contemplating new development or redevelopment should consult this plan** to ensure that development concepts are in-sync with the vision and goals of this plan.

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MONITORING SUCCESS

The true success of this plan will be realized through implementation. Therefore, the plan shall be continually monitored to ensure projects are properly positioned for funding and to keep priorities on track. Note that there are many factors in play, and priorities may change over time. While flexibility to respond to changes is necessary, projects should always seek to respond to the vision and goals, or “spirit”, of this plan.

A 6th Street Corridor Task Force should be organized, which could include members from the Steering Committee for this plan, in addition to the DDA and City staff and leadership. This task force should meet quarterly to check in on progress made, both from the private and public sectors, and discuss where partnerships make sense. This will also ensure transparency and inclusion.

HOW TO USE THIS CHAPTER

This chapter includes a master list of projects to implement, which are listed in a user-friendly spreadsheet. The spreadsheet is divided into “Administrative” and “Capital” projects, and projects are listed in order of their priority:

- Short-Term Projects (0-5 years)
- Mid-Term Projects (5-10 years)
- Long-Term Projects (10+ years)

NOTE:

Short-term projects (0-5 years) will begin after the Grand Avenue bridge project is expected to be completed, mid-year 2018.

LAND USE AND COMMUNITY AND ECONOMIC DEVELOPMENT PROJECTS

ACTION	TIMING	POTENTIAL PARTNERS	FUNDING
Rezoning			
<i>Rezone applicable properties within study area to coordinate with Development Code re-drafting</i>	Short-term	DDA; Community Development	General Fund
Historic District Formation			
<i>Meet with property owners within proposed historic district.</i>	Short-term	Historic Preservation Commission; Frontier Museum; Community Development	N/A
<i>Contract with consultant to prepare National Register District nomination, pending approval from majority of property owners.</i>	Mid-term	Community Development	State Historical Fund Grant / General Fund
Taxing District			
<i>Contract with consultant to evaluate formation of a new taxing district such as a BID or GID; evaluate implementation of an Attractions Tax, or other mechanisms to finance capital improvements within the study area; to evaluate extension of the Tax Increment Finance District.</i>	Short-term	DDA; City Manager; Community Development; Finance; Business/property owners	General Fund

INFRASTRUCTURE PROJECTS

ACTION	TIMING	POTENTIAL PARTNERS	POTENTIAL FUNDING
Devolution of 6th Street			
<i>Devolve Laurel to Pine.</i>	Short-term	Public Works, Engineering, City Manager, DDA, City Council, CDOT	N/A
<i>Assess Laurel to Devereux Road maintenance issues with CDOT.</i>	Short-term		
<i>Consider devolution Laurel to Devereux pending outcome of road maintenance assessment.</i>	Mid-term		
6th Street Streetscape Improvements - Devereux to Vapor Caves			
<i>Stripe 6th Street, Laurel to Pine, for on-street parking and bike lanes.</i>	Short-term	DDA; Engineering	TBD
<i>Create 90% construction plans for 6th Street improvements from Devereux to Vapor Caves; include Devereux and Highway 6 roundabout; phasing plan; construction estimates per phase; identify funding sources</i>	Mid-term	DDA; Engineering; Community Development	TBD
New Public Area at North Landing Pedestrian Bridge			
<i>Determine extent of north landing; contract with landscape architect and civil engineer to create a design plan and cost estimates</i>	Short-term	DDA; Engineering; Community Development; Public Works; City Manager; Parks & Recreation; Private Partnerships	TBD (utilize leftover funds reserved from this Plan)
<i>Develop 100% construction drawings; identify funding sources; secure funding.</i>	Mid-Term	DDA; Engineering; Community Development; Public Works; City Manager; Parks & Recreation; Private Partnerships	TBD; Public-Private Partnership
<i>Construct improvements</i>	Mid-term	DDA; Parks & Recreation; Engineering	TBD; Public-Private Partnership
Construct New Sidewalks			
<i>Olive; Pine; Maple; 5th; Laurel (west side); Linden (south side); West 1st Street</i>	Long-term	Engineering	A & I
Construct Bicycle Sharrows			
<i>6th Street (Olive Street to Vapor Caves) and Devereux Road (Hwy 6 to Centennial)</i>	Short-term	Engineering	Street Tax
<i>Linden and Laurel (new interchange to Hwy 6)</i>	Mid-term		
Devereux Road Improvements			
<i>Develop a plan and cost estimates to complete the sidewalk/path system along Devereux Road between Highway 6 and Centennial Street</i>	Short-term	Engineering	Street Tax
<i>Create 70% construction drawings for Devereux Road improvements based on plan recommendations</i>	Short-term		
<i>Construct improvements</i>	Mid-term		A & I
Devereux-Midland Bridge			
<i>Create 10% construction plans; include cost estimate</i>	Short-term	Engineering	Street Tax
<i>Create 100% construction drawings pending Council prioritization with other infrastructure projects.</i>	Mid-term		
<i>Construct bridge pending Council prioritization and funding</i>	Long-term		A & I

INFRASTRUCTURE PROJECTS, CONTINUED

ACTION	TIMING	POTENTIAL PARTNERS	POTENTIAL FUNDING
Parking Garage			
<i>Plan for north side public garage by selecting location, identifying funding sources, and considering construction timing. Coordinate with Area Wide planning efforts.</i>	Short-term	DDA; Community Development; Engineering; City Manager; Private property owners	Grant
<i>Construct garage</i>	Long-term		Public-Private Partnership
I-70 Pedestrian Bridge			
<i>Assess feasibility of and need for constructing a new pedestrian bridge over I-70. Bridge is dependent on construction of a public parking garage south of W. 1st Street adjacent to Interstate.</i>	Long-term	DDA; Community Development; Engineering	TBD
Bicycle/Pedestrian Loop			
<i>Create and sign a 1.75-mile bike/ped loop utilizing existing multi-use paths and city-owned rights-of-way. Coordinate timing of infrastructure improvements along Devereux Road and Highway 6. Promote the loop in trail maps and on city website.</i>	Short-term	Engineering; Transportation	Grant
<i>Create a secondary outer bike/ped loop. Coordinate with timing of infrastructure improvements including Midland-Devereux Bridge; new ped bridge over I-70; extension of 5th Street</i>	Long-term	Engineering; Transportation	TBD